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File ID: 2026-00879

4/28/2026

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**[Appeal] Florin Road Quick Quack Car Wash (P25-013) [Noticed 04/17/2026; Published 04/17/2026]**

File ID: 2026-00879

**Location:** 3815 Florin Road, APN: 041-0120-022-0000; District 5

**Recommendation:** Conduct a public hearing and upon conclusion: adopt a **Resolution** denying a Conditional Use Permit to establish a car wash within the General Commercial (C-2) Zone and Site Plan and Design Review for the construction of a car wash and associated site improvements on a ±1.15-acre portion of a vacant ±14.25-acre parcel.

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**Presenter:** Danny Abbes, Associate Planner, (916) 808-5873, dabbes@cityofsacramento.org, Department of Community Development

**Applicant:** Vance Shannon, 6020 W Oak Blvd., Suite 300, Rocklin, CA 95765

**Property Owner:** Ethan Conrad, 1300 National Dr., Suite 100, Sacramento, CA 95834

**Attachments:**

- 1-Description/Analysis
- 2-Background
- 3-Resolution: Findings of Fact denying project entitlements
- 4-Proposed Project Plans
- 5-Civic Thread project comment letter
- 6-Sacramento Metropolitan Air Quality Management District project response
- 7-General Plan Map M-3: Candidate High-Frequency Transit Corridors
- 8-General Plan Map EJ-5: Communities Cumulatively Impacted by Environmental Justice Issues
- 9-General Plan Map M-1: Roadway Reallocations
- 10-General Plan Map EJ-3: Census Tracts with Highest Pollution Burden Score
- 11-General Plan Map CP-SA-3: South Area Maximum Floor Area Ratio
- 12-Property owner's conceptual plan for potential future development of the remainder of the site's

parcel and adjoining parcels  
13-Project Draft Conditions  
14-Applicant Presentation  
15-Staff Presentation  
16-Project Comments and Outreach Memo Submitted by Applicants  
17-Appeal

### **Description/Analysis**

**Issue Detail:** The applicant is proposing to establish a car wash on a ±1.15-acre portion of a larger ±14.2-acre parcel within the General Commercial (C-2) zone. The request requires approval of a Conditional Use Permit to establish a car wash, and Site Plan and Design Review for construction of the building and associated site improvements.

Staff does not support the request to establish a car wash use at this location. Staff recommend that the City Council deny the project for the following reasons:

- The proposed car wash is inconsistent with General Plan and South Area Community Plan policies, Issues and Opportunities, and visions related to transit-supportive development (i.e. housing and mixed-use) at the site.
- A car wash land use would fail to take advantage of a site that is well-positioned to serve residents who could conveniently walk, bike, or take transit to daily destinations and customers who could arrive by foot, bike, or transit.
- A non-auto oriented use at this location could reduce vehicle trips and the number of collisions within the Florin Road High Injury Network and adjacent Vision Zero Top 5 Corridor.
- The proposed car wash would not add a needed service to the area.

The following Policy Considerations and Entitlement sections of this report discuss these and other concerns in further detail.

Staff received an appeal of the Planning and Design Commission's denial of the project on March 4, 2026, that claims project approval is supported by substantial evidence. Staff's recommendation to deny the project remains unchanged.

**Public/Neighborhood Outreach and Comments:** As part of the application review process, the proposal was routed to Civic Thread, Sacramento Area Bicycle Advocates, Sacramento Regional Transit, Sacramento Metropolitan Air Quality Management District, Preservation Sacramento, North

Laguna Creek Valley Hi Community Association, and Florin Road Partnership.

Staff received comments from Civic Thread and Sacramento Metropolitan Air Quality Management District (SMAQMD). Civic Thread stated that the project is inconsistent with city-adopted multimodal, safety, and climate goals, and recommends project denial. SMAQMD's response to the proposed project states that a pedestrian-oriented use, rather than an automobile-oriented use, would be favorable at the project location as it would be consistent with the local public health aims of the South Sacramento-Florin AB 617 Community Air Protection Program and support sustainable transportation. Civic Thread's and SMAQMD's full comments are included in Attachments 5 and 6 of this report. Staff did not directly receive any project comment letters from individual members of the public. The applicant submitted comment letters and an outreach memo which can be seen in Attachment 16.

Notification of this hearing was sent to property owners and occupants within a 500-foot radius of the subject site, and the subject site was posted with contact information for project questions, as well as the date, time, and participation details for the project hearing.

### **Policy Considerations:**

#### 2040 General Plan

The 2040 General Plan designation for the subject site is Residential Mixed Use (RMU). This designation is intended to foster vibrant, walkable areas with a high-intensity mix of residential, commercial, office, and public uses, where daily errands can be accomplished on foot, by bicycle, or by transit.

Allowable uses include the following:

- A full range of residential, retail, employment, entertainment, cultural, and personal service uses
- General offices and community institutional uses, such as banks, financial institutions, care facilities, and medical and professional offices
- Assembly facilities
- Compatible public, quasi-public, and special uses

***Staff Response:** A car wash is not a precluded use within the RMU designation; however, given the context of its proposed site, which is surrounded by a continuous system of sidewalks/crosswalks and within walking and biking distance of residential, commercial, office, schools, and transit, a use that exclusively serves customers with cars at this location is an ill-suited use to foster walkability and*

*does not allow for errands to be accomplished on foot, by bicycle, or by transit.*

Staff considered potentially applicable General Plan and South Area Community Plan policies and Community Issues and Opportunities including South Area Issue and Opportunity - Infill Development, and General Plan Land Use Policy 1.8 (Jobs and Housing Balance):

### Infill Development

**South Area Community Issue and Opportunity: Infill Development** - While most of the South Area is fully built out in established neighborhoods and commercial areas, vacant lots near Cosumnes River College, along commercial corridors such as Mack Road, and in the Delta Shores development, and at the City-owned 102-acre parcel, as well as underutilized lots, such as those along Florin Road, represent opportunities for infill development that can help realize the community vision. Development in these areas can support existing community-serving businesses like grocery stores and mercados, helping new ones to establish and creating new jobs for residents by building on the area's strength as healthcare and education centers. Many of these vacant or underutilized parcels have excellent access to light rail, providing direct access to Downtown. *(underline added by staff for emphasis)*

Staff Response: *The South Area Community Plan speaks specifically to vacant properties along high frequency bus routes as an opportunity for transit-oriented development:*

*"In the South Area, the Sacramento Regional Transit (SacRT) Blue Line light rail and high frequency bus routes create opportunities for transit-oriented infill development of vacant and underutilized properties. Land use designations in these transit-oriented development areas are intended to promote mixed-use areas near light rail and bus service to incentivize multi-unit housing that supports more frequent/reliable transit service and bolsters retail vitality." (Community Plan: South Area 11-SA-7)*

*Furthermore, The Community Vision section of the South Area Community Plan states:*

*"Vacant and underutilized land has been redeveloped into vibrant mixed-use neighborhoods that feature a variety of affordable and adaptable housing options and support high-quality transit service along the SacRT Blue Line light rail and the Florin Road high-frequency bus corridor." (Community Plan: South Area 11-SA-3)*

*Staff finds that the proposed auto-oriented infill development does not realize the South Area community vision of mixed-use, transit-oriented infill development along Florin Road near quality existing and planned further enhanced bus service.*

Staff had the following further considerations regarding the impacts of infill development:

- *The proposed land use is an abundantly available service within the area. There are already four existing car washes less than one mile from the proposed site.*
- *The proposed site and structure have limited potential for adaptable re-use, due to minimal commercial/office area and a very high percentage of space dedicated to vehicles. Staff believe it is unlikely the building could accommodate a future business that is transit supportive without cost-prohibitive improvements.*
- *The proposed use is auto-oriented rather than transit-oriented and does not offer a high level of development intensity. The General Plan designates the site as part of a Transit Oriented Development Area (General Plan Map M-3, Attachment 7) and as one of the few selected areas of the highest development intensity in the South Area (General Plan Map CP-SA-3, Attachment 11). The 1.15-acre site would require a minimum of 23 units for a housing development application and allows for a commercial/office development to have a building of over 200,000 square feet.*
- *The site already has sidewalk infrastructure that connects to the surrounding sidewalk system. While the proposed project could provide a separated sidewalk and trees along its frontage, this will be a requirement for any type of development at the site. The short-term gain would be outweighed by both short and long-term consequences as described in the remainder of the report.*
- *Approval of the proposal could set precedent to a pattern of auto-oriented uses for the remainder of the large surrounding ±14.2-acre vacant area, and the adjacent ±0.52 corner parcel, which further jeopardizes area pedestrian safety, decreases the effectiveness of local transit investments and services, and encourages increased use of personal vehicles which adversely impacts local air quality. (See Attachment 12 which is an exhibit referred to in the project's application as the "[Property] owner's conceptual plan for potential future development of the remainder of the site's parcel and adjoining parcels.")*

### Jobs and Housing Balance

**LUP-1.8 Jobs-Housing Balance.** The City shall encourage a balance between job type, the workforce, and housing development to reduce the negative impacts of long commutes and provide a range of employment opportunities for all city residents.

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*Staff Response: An express car wash is a relatively low-volume employer which would contribute minimally to local commuting or to the existing high-ridership bus service in the immediate area. As an exterior only express carwash, fewer employees per shift are utilized compared to a full-service carwash. In addition to a low volume of employment, it's typical that most car wash job positions do not offer living wages, healthcare benefits, or full-time employment. The census tract in which the site is located ranks in the 4<sup>th</sup> percentile of residents above poverty level, and 15<sup>th</sup> percentile of residents with access to healthcare. (Source: California Healthy Places Index, 2015-2019 American Community Survey) Furthermore, the jobs may displace existing similar jobs within less than a mile from the site due to market saturation.*

*Staff finds that the proposed use offers a relatively low benefit related to the local jobs-housing balance.*

Staff find that the proposed project is inconsistent with the following 2040 General Plan Land Use Element Goals and policies supporting transit-friendly development, transportation safety, and community air protection. Further evaluation and impacts related to the following policies can be found in the Entitlements section of this report.

**Goal LUP-4** Walkable, transit-oriented centers and corridors that concentrate new jobs, housing, and entertainment opportunities to support frequent, reliable transit service and foster connected, accessible neighborhoods.

**Policy LUP-4.1 Transit-Supportive Development.** The City shall encourage increased residential and commercial development intensity within ½-mile of existing high-frequency bus stops and existing and planned light rail stations, bus rapid transit stations, and commuter rail stations to support more frequent, reliable transit service and vibrant, walkable neighborhoods.

*Staff Response: The proposal is a vehicle-dependent commercial development with a low-volume employer located approximately 100 feet from an existing bus stop on a high-ridership route and 30-minute service intervals, and along the planned service route of future bus rapid transit.*

**Goal M-4** A safer transportation system.

**Policy M-4.3 Vision Zero.** The City shall utilize a data-driven, “vision zero” approach to eliminate all traffic fatalities and severe injuries by 2027, while increasing safety, health, and equitable mobility for all.

**Policy M-4.9 Safe Routes to School.** The City shall assess opportunities to develop and support Safe Routes to School programming.

*Staff Response: The project site is a quarter-mile walking distance from Luther Burbank High School, also located on Florin Road, and a half-mile walking distance from both Bowling Green Elementary School and Fern Bacon Middle School in an area with a low rate of household vehicle ownership where many residents rely on walking and public transit. The site is well-positioned within a walkable, transit-accessible neighborhood and zoned to accommodate a multitude of uses that could generate a high number of trips by pedestrians, cyclists, and transit.*

*This section of Florin Road is part of a High Injury Network right next to a Top 5 Vision Zero Corridor and adjacent to a transit stop, meaning City statistical data indicates that this collection of circumstances makes the roadway in the immediate project area among the very highest risk for traffic fatality and severe injury in the city. The proposed car wash would only serve customers with cars, in an area extremely vulnerable to vehicle-related injury and death. Staff believe auto-oriented uses such as a car wash do not belong at this site.*

**Goal EJ-1** Clean air water, and soil with no segment of the community disproportionately burdened by environment conditions.

**Policy EJ-1.2 Community Air Protection.** On an ongoing basis, the City shall support the Sacramento Metropolitan Air Quality Management District (SMAQMD), community members, businesses, and other stakeholders in implementation of AB 617 and other Community Emissions Reduction Programs (CERPs), which may include developing and implementing community air monitoring plans, community emissions reduction plans, and other air pollution control initiatives.

*Staff Response: The Sacramento Metropolitan Air Quality Management District (SMAQMD) reviewed the proposed project and stated that “ideally the site location on a prominent corner in this area would be a pedestrian-oriented use rather than an automobile-oriented use. This would be consistent with the public health aims of AB 617 and would support sustainable transportation investments such as Bus Rapid Transit (BRT).” Denial of the proposed car wash at this location supports SMAQMD and upholds this general plan policy aimed at helping reduce localized emissions.*

#### South Area Community Plan

The subject property is located within the South Area Community Plan area of the General Plan which encompasses 16.5 square miles of the city south of Fruitridge Road to the southern city border mostly between Highway 99 and Interstate 5.

The plan includes a vision and policies derived from extensive public input and relevant South Area plans and studies. These policies are specific to the South Area and intend to supplement citywide policies. The plan also contains identified community issues and opportunities, associated with citywide General Plan policies, expected to be considered and addressed with future area development. Staff find the proposed project inconsistent with the following vision, policy, and

community issues/opportunities discussion within the South Area Community Plan:

**Vision:** “Vacant and underutilized land has been redeveloped into vibrant mixed-use neighborhoods that feature a variety of affordable and adaptable housing options and support high-quality transit service along the SacRT Blue Line light rail and the Florin Road high-frequency bus corridor.”

*Staff Response: The proposed development does not provide housing or mixed-use and does not support high quality transit service along Florin Road.*

**Policy SA-M-1 High Frequency Transit.** The City should collaborate with the Sacramento Regional Transit District (SacRT) to plan and implement high-frequency transit between Indian Lane and Franklin Boulevard, consistent with the Sacramento Regional Transit Master Plan and to the satisfaction of the City’s Department of Public Works.

*Staff Response: The project site is located on Florin Road between Indian Lane and Franklin Boulevard. The city has designated the site as part of a planned Transit-Oriented Development Area to complement SacRT’s quality transit system along Florin Road. Transit-friendly uses combined with transit access help the city reduce vehicle congestion, improve air quality, and reach climate action goals. The pairing of transit-friendly uses with transit access also helps provide more equitable access to goods, services, and housing to Sacramentans who rely upon transit as a primary mode of transportation.*

**South Area Community Plan Issues and Opportunities: High Frequency Transit Access** - The recent expansion of the Blue Line to Cosumnes River College could be complemented by the potential for a Bus Rapid Transit (BRT) line on Florin Road. Intensifying residential, mixed-use, and office mixed-use development around stations supporting walking connections to and from stations can help to promote higher transit use and reduce local congestion and air pollution.

*Staff Response: The proposed development is an auto-oriented use that does not promote higher transit use and does not offer commercial service to pedestrians or cyclists, thereby eliminating the best opportunity to reduce local congestion and air pollution.*

**South Area Community Plan Issues and Opportunities: Air Quality** - Parts of the South Area, particularly low-income neighborhoods, face high air pollution exposure. The California Air Resources Board and the Sacramento Metropolitan Air Quality Management District are working with communities in the South Sacramento-Florin and Meadowview communities to develop community air monitoring and emissions reduction plans. This community plan offers

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further opportunities for targeted policies that promote the health and well-being of residents.

*Staff Response: The project site is within a low-income neighborhood exposed to high air pollution. Developing a site capable of conveniently serving non-polluting forms of transportation with an auto-oriented use would exacerbate the stated issue and pass upon a prime opportunity.*

### Climate Action and Adaptation Plan (CAAP)

Given the site's high level of access to transit, pedestrian infrastructure, schools, and mix of surrounding land uses and amenities, the proposed project is inconsistent with the CAAP's following Transportation Strategy and Action and Implementation Measure:

#### *Transportation Strategy*

Greenhouse Gas (GHG) emissions from the transportation sector are the largest contributor to Sacramento's GHG emissions and are driven by on-road passenger vehicles, heavy duty vehicles and commercial vehicles. Getting to carbon neutral in this sector entails a three-tiered approach:

- Significantly increase the portion of trips completed via active transportation options like walking and biking
- Transition the majority of remaining trips to public transit and carpools
- Transition any remaining passenger and vehicle trips to EVs.

#### *Actions and Implementation*

**Measure TR-2.3** Encourage SacRT to provide frequent, reliable transit in the City's priority corridors to reduce Vehicle Miles Traveled (VMT) and support SacRT in implementing priority transit corridors. Coordinate transit priority corridors with consideration of transportation needs as well as land use planning to provide transit-supportive land uses. Encourage the expansion of frequent, reliable transit services throughout the City.

*Staff Response: An auto-oriented use located at a walkable, transit-accessible site discourages active transportation and does not support transit.*

*Federal and State transportation grants are a key component of funding for transit services such as Bus Rapid Transit. An example is California's Interregional Transportation Improvement Program (ITIP.) The ITIP focuses on projects that align with the Climate Action Plan for Transportation Infrastructure, supporting GHG reduction, equity for under-resourced communities, and interregional travel. Two 2026 ITIP Scoring and Selection criteria categories related to land use decisions are:*

*Community & Environmental Impact: Emphasis on projects that improve public health, address community needs (equity), and reduce greenhouse gas emissions.*

*Technical Criteria: Evaluation includes cost-effectiveness, congestion reduction, and improvement of interregional movement for people and goods throughout California on the State Highway System and development of Intercity Passenger Rail Corridors.*

### Housing Element Sites Inventory

This project is located on a consolidated site listed in the 2021-2029 Regional Housing Needs Assessment (RHNA) Housing Element Sites Inventory, which anticipates 755 total units including 377 units of moderate income housing and 378 units of lower income housing. This project does not propose housing units.

**Environmental Considerations:** The California Environmental Quality Act (CEQA) does not apply to projects which a public agency rejects or disapproves pursuant to Guidelines Section 15270(a).

Although prior to City Staff's decision to recommend denial of the project, a Modified Initial Study/15183 Checklist was prepared to analyze if the proposed project would not result in any significant impacts that:

- 1) were peculiar to the project or the project site;
- 2) were not identified as a significant effect in the General Plan Master Environmental Impact Report (MEIR);
- 3) were potentially significant off-site impacts and cumulative impacts which were not discussed in the prior MEIR prepared for the General Plan; or
- 4) were previously identified significant effects, and which as a result of substantial new information that was not known at the time that the General Plan MEIR was certified, were determined to have a more severe adverse impact than discussed in the General Plan MEIR.

Under Section 15183 of the CEQA Guidelines, where a project is consistent with the development density established for a property under an existing general plan or zoning ordinance for which the City already certified an Environmental Impact Report (EIR), additional environmental review is not required "except as might be necessary to examine whether there are project-specific significant effects which are peculiar to the project or its site."

On February 27, 2024, the City of Sacramento adopted the 2040 General Plan, which became effective on March 28, 2024. The City of Sacramento also certified a Master Environmental Impact Report (MEIR) associated with the 2040 General Plan on February 27, 2024. The General Plan

MEIR is a master EIR, prepared pursuant to Section 15169 of the CEQA Guidelines (Title 14, California Code of Regulations §15000 et seq.). The General Plan MEIR analyzed full implementation of the General Plan and identified measures to mitigate the significant adverse impacts associated with the General Plan to the maximum extent feasible.

The Environmental Services Manager has concluded, based upon the analysis and conclusions of the Modified Initial Study/15183 Checklist, the project would be exempt from further review pursuant to CEQA Guidelines Section 15183.

The Initial Study/15183 Checklist is available on the City's EIR webpage at:

<https://www.cityofsacramento.gov/community-development/planning/environmental/impact-reports>

**Economic Impacts:** Not applicable.

**Sustainability:** The proposed project does not support a sustainable development pattern in the city. It occupies a walkable urban space near schools, neighborhood services, stores, and transit connections with a car-dependent business. Housing or pedestrian and transit-oriented commercial development are allowed uses at the site that would contribute to a sustainable development pattern.

**Commission/Committee Action:** On February 26, 2026, the Planning and Design Commission conducted a public hearing and denied the Conditional Use Permit and Site Plan and Design Review entitlements to construct and operate the proposed car wash.

**Rationale for Recommendation:** Staff recommend the Council deny the requested entitlements to permit a car wash based on the findings of fact listed within Attachment 3. The proposed project does not align with goals and land use policies of the 2040 General Plan, South Area Community Plan Issues and Opportunities, Vision Zero, the Climate Action and Adaptation Plan, and required fact findings for approval of a Conditional Use Permit.

**Financial Considerations:** Not applicable.

**Local Business Enterprise (LBE):** Not applicable.

## Background Information and Surrounding Land Use Context

The project is proposed on a 1.15-acre site along Florin Road about 300 feet east of Franklin Boulevard. It is a portion of a larger 14-acre vacant site that was formerly used for automobile sales from the 1960s to 2013. The surrounding area contains a variety of uses including housing, longtime auto-oriented facilities, schools, grocers, restaurants, retail, and commercial and community services.

As large auto dealerships have transitioned away from the Florin Road Corridor, the general area has seen new, neighborhood serving uses move in:

- The warehouse/office complex immediately north of the project site now includes multiple fitness-oriented uses, churches, an adult day center, and other community programming (7000 Franklin Boulevard; #1 on the map below).
- In 2025, the Sacramento Native American Health Center remodeled and occupied a 38,000 square foot building of a former automobile service center at Florin Road and Luther Drive (3800 Florin Road; #2 on below map).
- The Kind South, a new 108-unit apartment complex, will open on a formerly vacant site on Florin Road and Woodbine Avenue in February 2026 (7141 Woodbine Avenue; #3 on below map).

**Figure 1: Aerial view of subject site via Google (North at top)**



**Table 1: Project Information**

<b>General Plan Designation</b>	Residential Mixed Use
<b>Zoning</b>	General Commercial (C-2)
<b>Property Area</b>	±1.15 acres
<b>Current Use</b>	Vacant
<b>Proposed Use</b>	Car wash
<b>Building Area</b>	±3,337 square feet
<b>Proposed Hours of Operation</b>	7am to 9pm daily
<b>Anticipated Employees</b>	15

## Entitlement Review

Auto-Service Conditional Use Permit: A Conditional Use Permit (CUP) is required to establish a car wash use within the General Commercial (C-2) zone. The CUP process is designed to assess a specific proposed use and evaluate its potential impacts to the surrounding area.

### Local Land Uses

There are four existing car washes less than a mile from the proposed car wash site, including a car wash directly across the street. Figure 2 below shows an approximately one-mile stretch of Florin Road with the proposed site and four active nearby car washes.

**Figure 2: Florin Road Car Washes Relative to Proposed Car Wash Site**



- (#1) Magic Car Wash & Detail – 2813 Florin Road – 0.8 miles away
- (#2) Magic Wand Car Wash – 3465 Florin Road – 0.35 miles away
- (#3) Shell Car Wash – 4050 Florin Road – 0.03 miles away
- (#4) AMPM car wash – 4421 Florin Road – 0.32 miles away

Car washes are an already abundantly available service in the area. Staff does not find that a fifth car wash within a mile would add needed convenience to the neighborhood.

(Staff notes that Sacramento County Planning also has an active entitlement application for a Quick Quack car wash on Florin Road, at the intersection of 65<sup>th</sup> Street on the other side of Highway 99, approximately 1 mile east of the subject site.)

This proposed car wash site has a walk score and bike score of 76 and 74, respectively, categorizing it as very walkable, very bikeable and not car dependent. The area hosts a rich mix of land uses including housing, several schools (Bowling Green Elementary, Fern Bacon Middle, Luther Burbank High), resources (i.e. South Sacramento Family Resource Center, Native American Health Center), grocery stores, restaurants, retail, banks, and other services. The variety of uses within close distance could allow for many customers or residents of the site to arrive by foot or bike; however, every customer of a car wash would have to be by vehicle trip, reducing the site's highly walkable potential for the local community. Non-auto-oriented commercial or housing – uses allowed by right at the site – could take advantage of the very walkable location.

### *Relevant Community Characteristics*

- The proposed car wash is in a census tract where greater than 17 percent of households do not own a single vehicle, and most households do not own a second vehicle. Vehicle access is in the 7<sup>th</sup> percentile ranking in the state. *(Source: California Healthy Places Index, 2015-2019 American Community Survey)*
- Just 34 percent of residents within the subject census tract are above poverty, which is in the 4<sup>th</sup> percentile ranking in the state. *(Source: California Healthy Places Index, 2015-2019 American Community Survey)*
- The site is within a SB 535 Disadvantaged Community, which is a designation of an area disproportionately affected by pollution, poverty, and health risks, and designated to receive funds to assist in pollution reduction and climate.
- The site is within the South Sacramento-Florin AB 617 Community, which is a neighborhood with a disproportionately high air pollution burden that is targeted for emission reductions.
- The site is within an area considered to be Most Disadvantaged by the City's General Plan, Map EJ-5: Communities Cumulatively Impacted by Environmental Justice Issues (Attachment 8). The determination considers a collection of 68 factors including pollution exposure, transportation safety, and poverty.

### *Vision Zero*

“Vision Zero” is a city-adopted initiative to prevent loss of life and significant injury resulting from auto accidents, particularly automobile accidents involving pedestrians who are much more likely to be killed or seriously injured.

The project site is in a neighborhood with low vehicle ownership, three schools, and located along the “High Injury Network” next to a Top 5 Vision Zero Corridor.

The High Injury Network is a group of designated corridors “with the highest levels of fatal and serious crashes for pedestrians, bicyclists, and motorists.” Vision Zero data breaks down the High Injury Network further into focused sections of Top 5 and Top 10 Corridors where the highest of the high rates of death and serious injury occur. The project site is less than 200 feet from the boundary of a Top 5 Corridor and transit stop.

**Figure 3: Vision Zero Traffic Statistics from the City’s General Plan Mobility Element**

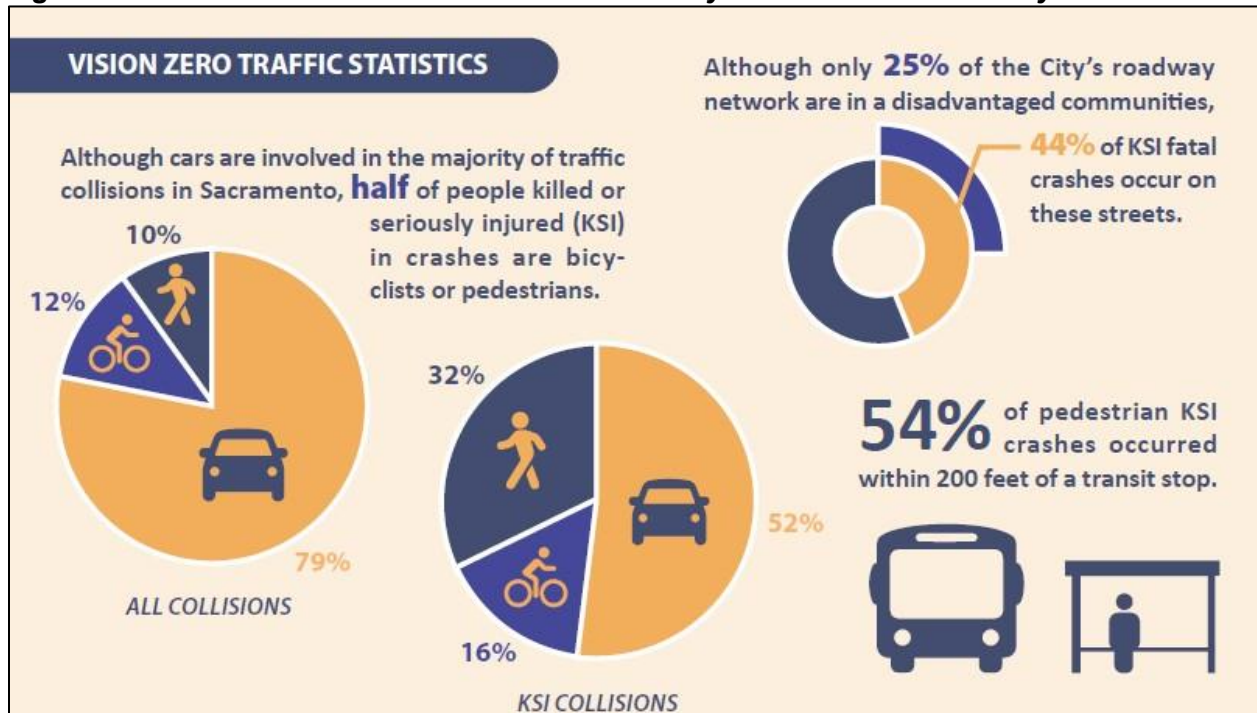


Figure 3 above from the General Plan provides Vision Zero statistics based on city data. The site is located within a walkable, disadvantaged community near the Florin Road and Franklin Boulevard westbound pedestrian shelter for high ridership bus route 81. Vehicles would exit the proposed site from a driveway approximately 270 feet from the shelter and be making a right turn maneuver and accelerating and integrating into the flow of traffic toward the direction of the bus stop.

A non-auto oriented use at the subject location, especially given its walkable and transit-accessible surroundings, could reduce vehicle trips and the number of collisions within the Florin Road High Injury Network and adjacent Top 5 Corridor.

*Transit Proximity*

The Florin Road and Franklin Boulevard Westbound bus stop next to the site provides a direct connection to the Florin Light Rail Station. Across the street, the Florin Road and Munson Way Eastbound bus stop provides a direct drop off from the light rail station. It is a 0.67-mile walking distance from the proposed car wash site to the light rail station.

The area surrounding the site has excellent existing local and regional transit access, including nearby bus routes 81 along Florin Road (noted as one of Sacramento Regional Transit (SacRT)'s highest ridership corridors) and 67 along Franklin Boulevard, each offering service every 30 minutes. Five bus stops less than a quarter of a mile from the site offer service in all directions. It is poor planning practice to place a commercial use with low-intensity employment that does not serve pedestrian customers near robust and frequent transit service.

SacRT is planning to further increase service in this area, making it part of a Bus Rapid Transit (BRT) Corridor along Florin Road. Funding for the Florin Road BRT Corridor has not yet been identified to implement a timeline of service, but SacRT's vision to provide BRT in the future here has not changed. The City's General Plan also honors and maintains SacRT's vision of BRT in this area, officially designating it as a Candidate High-Frequency Transit Corridor. (Attachment 7, General Plan Map M-3)

### *Community Air Protection*

As previously noted in the Community Characteristics section, the project site is located within the boundaries of both an AB 617 community and SB 535 community, which highlights its elevated exposure to air pollutants and toxic air contaminants. The Environmental Justice element of the City's General Plan also designates the site and surrounding community as enduring a high pollution burden (Attachment 10, General Plan Map EJ-3: Census Tracts with Highest Pollution Burden Score).

While none of the designations disallow auto-oriented uses, they do provide a background for consideration. Approval of a discretionary use permit for a low-intensity commercial use that does not provide services to customers who arrive by foot, bike, or transit within a very walkable, transit-friendly area is not in line with established environmental justice aims to reduce the disproportionate pollution burden of the neighborhood. A wide variety of uses permitted by-right at the site, including retail, restaurant, office, and housing, would allow customers or residents of the site convenient access by foot, bike or transit, all methods which contribute little to nothing towards air pollution relative to the high contribution of personal vehicles.

### Site Plan and Design Review:

#### *General Plan Evaluation*

Staff find that the proposed land use and site design is inconsistent with the goals and policies of the 2040 General Plan and South Sacramento Community Plan. The subject property has surrounding access to pedestrian infrastructure, transit service, and a mix of uses which make it well-positioned to foster walkability and provide support for errands to be accomplished without the use of a personal vehicle. The auto-oriented design and layout of the site is dominated by vehicle circulation, maneuvering, and parking. Furthermore, the on-site building does not have any public pedestrian entrances or offer any usefulness to site visitors who could arrive via foot/bike/transit.

## Development Standards

The proposed project meets the applicable building height, setback and parking development standards of the C-2 Zone, as shown in Table 2 below.

**Table 1: C-2 Development Standards**

Standard	Required	Proposed	Deviation
Front Setback (South)	No minimum	±170 feet	No
Interior-side Setback (East)	No minimum	±105 feet	No
Interior-side Setback (West)	No minimum	±60 feet	No
Rear Setback (North)	No Minimum	±60 feet	No
Building Height	Maximum 45'	±29 feet	No
Vehicle Parking	None	22 spaces	No
Shade Coverage	50%	51%	No
Floor-Area Ratio*	0.4 – 4.0	±0.06	No

\*General Plan Policy LUP 3.8 exempts development that normally conducts a substantial amount of its operations outdoors from minimum FAR standards.

**Figure 4: Rendering of proposed site from Florin Road**



## Building

The car wash is a 3,337 square foot building with a floor plan including the car wash tunnel, equipment storage room, and an office, lounge, and restroom for employees. The primary exterior materials include painted stucco and split face masonry wall, with



## RESOLUTION NO. 2026-

Adopted by the Sacramento City Council

April 28, 2026

### **Denying project entitlements related to the Florin Road Quick Quack Car Wash Project (3815 Florin Road) based on Findings of Fact (P25-013) (APN: 0411-0120-022-0000)**

#### **Background**

- A. On February 26, 2026, the City Planning and Design Commission conducted a public hearing on and denied project entitlements related to the Florin Road Quick Quack Car Wash Project.
- B. On April 28, 2026, the City Council conducted a public hearing, for which notice was given under Sacramento City Code Section 17.812.010(2)(b).

#### **Based on the facts set forth in the Background, the City Council resolves as follows:**

Section 1. Based on the verbal and documentary evidence received at the hearing on the Florin Road Quick Quack Car Wash Project (Project), the City Council denies project entitlements based on the findings of fact as set forth below.

Section 2. The City Council denies the Project entitlements related to the Florin Road Quick Quack Car Wash based on the following findings of fact:

- A. **Environmental Determination:** Projects Which Are Disapproved pursuant to CEQA Guidelines section 15270(a):
  - 1. The project has been determined to be exempt from environmental review pursuant to California Environmental Quality Act (CEQA) Guidelines, § 15270(a). CEQA does not apply to projects which a public agency rejects or disapproves.

- B. The **Conditional Use Permit** to establish a car wash within the General Commercial (C-2) Zone is **denied** based on the following Findings of Fact:

*Findings of Fact under City Code §17.808.200.C.1.a, c and d:*

- a. The proposed use and its operating characteristics are inconsistent with the goals and policies that support the City of Sacramento 2040 General Plan Residential Mixed Use (RMU) land use designation. The RMU designation is intended to foster vibrant, walkable areas with a high-intensity mix of

residential, commercial, office, and public uses, where daily errands can be accomplished on foot, by bicycle, or by transit. Given the context of its proposed site, which is surrounded by a continuous system of sidewalks/crosswalks and within walking and biking distance of residential, commercial, office, schools, and transit, a car wash at this site would be contrary to the intent of the RMU designation. The use exclusively serves customers with cars and therefore does not foster walkability and does not allow for errands to be accomplished on foot, by bicycle, or by transit.

The project is inconsistent with several City of Sacramento 2040 General Plan goals and land use policies, as well as South Area Community Plan policy, issues, and opportunities, including Transit-Supportive Development (Goal LUP-4; Policy LUP-4.1), Vision Zero and Safe Routes to School (Goal M-4; Policy M-4.3, Policy M-4.9), Community Air Protection (Goal EJ-1; Policy EJ-1.2), High Frequency Transit (Policy SA-M-1), South Area Community Plan Issues and Opportunities: High Frequency Transit Access, and South Area Community Plan Issues and Opportunities: Air Quality.

- c. The car wash is not physically suitable on the project site as the site is located less than 200 feet from a bus stop that provides frequent and convenient local transit service. Sites located within such close distance are suitable for land uses that support and take advantage of the area's proximity to public transit. A car wash at this location does not support the long-term economic viability of transit service in the South Sacramento area and will not contribute significantly to future ridership.
  - d. The proposed use and its operating characteristics are detrimental to the public health, safety, convenience, or welfare of persons residing, working, visiting, or recreating in the surrounding neighborhood and will result in the creation of a nuisance in that the project site offers the surrounding neighborhood convenient access by foot, bike, and transit; however, a proposed car wash use removes this useful (and in some cases necessary) convenience for customers of a business at the site.
- C. Site Plan and Design Review for the construction of a car wash and associated site improvements on a ±1.15-acre portion of a vacant ±14.25-acre parcel is **denied** based on the following Findings of Fact:

*Findings of Fact under City Code §17.808.180.B.1 and B.4:*

- a. The design, layout, and physical characteristics of the proposed development are inconsistent with the goals and policies that support the City of Sacramento

2040 General Plan land use designation of Residential Mixed Use. This designation is intended to foster vibrant, walkable areas with a high-intensity mix of residential, commercial, office, and public uses, where daily errands can be accomplished on foot, by bicycle, or by transit. Given the context of the project site, which is surrounded by transit options, a continuous system of sidewalks/crosswalks, and within walking and biking distance of residential, commercial, office, schools, and public uses, the auto-oriented design and layout of the site (a site dominated by vehicle circulation, maneuvering, and parking, and a building with no public entrance for pedestrians) does not foster walkability and does not allow for errands to be accomplished on foot, by bicycle, or by transit.

The project is inconsistent with several City of Sacramento 2040 General Plan goals and land use policies, as well as South Area Community Plan policy, issues, and opportunities, including Transit-Supportive Development (Goal LUP-4; Policy LUP-4.1), Vision Zero and Safe Routes to School (Goal M-4; Policy M-4.3, Policy M-4.9), Community Air Protection (Goal EJ-1; Policy EJ-1.2), High Frequency Transit (Policy SA-M-1), South Area Community Plan Issues and Opportunities: High Frequency Transit Access, and South Area Community Plan Issues and Opportunities: Air Quality.

- b. The design, layout, and physical characteristics of the proposed development, which include a high proportion of space for vehicle circulation, maneuvering, and parking, and a building with no public entrance for pedestrians, are not functionally compatible with the surrounding neighborhood that is walkable and highly transit-accessible.



**APPLICANT:**

QUICK QUACK CARWASH  
6020 WEST OAKS BLVD., SUITE 300  
ROCKLIN, CA 95765

**OWNER:**

ETHAN CONRAD  
1300 NATIONAL DRIVE, SUITE 100  
SACRAMENTO, CA 95834  
TEL: (916) 779-1000  
EMAIL: ETHAN@ETHANCONRADPROP.COM

**ENGINEER:**

BARGHAUSEN CONSULTING ENGINEERS, INC  
18215 72ND AVENUE SOUTH  
KENT, WA 98032  
TEL: (425) 251-6222  
EMAIL: KHELD@BARGHAUSEN.COM  
CONTACT: KACEY HELD, PE

**ARCHITECT:**

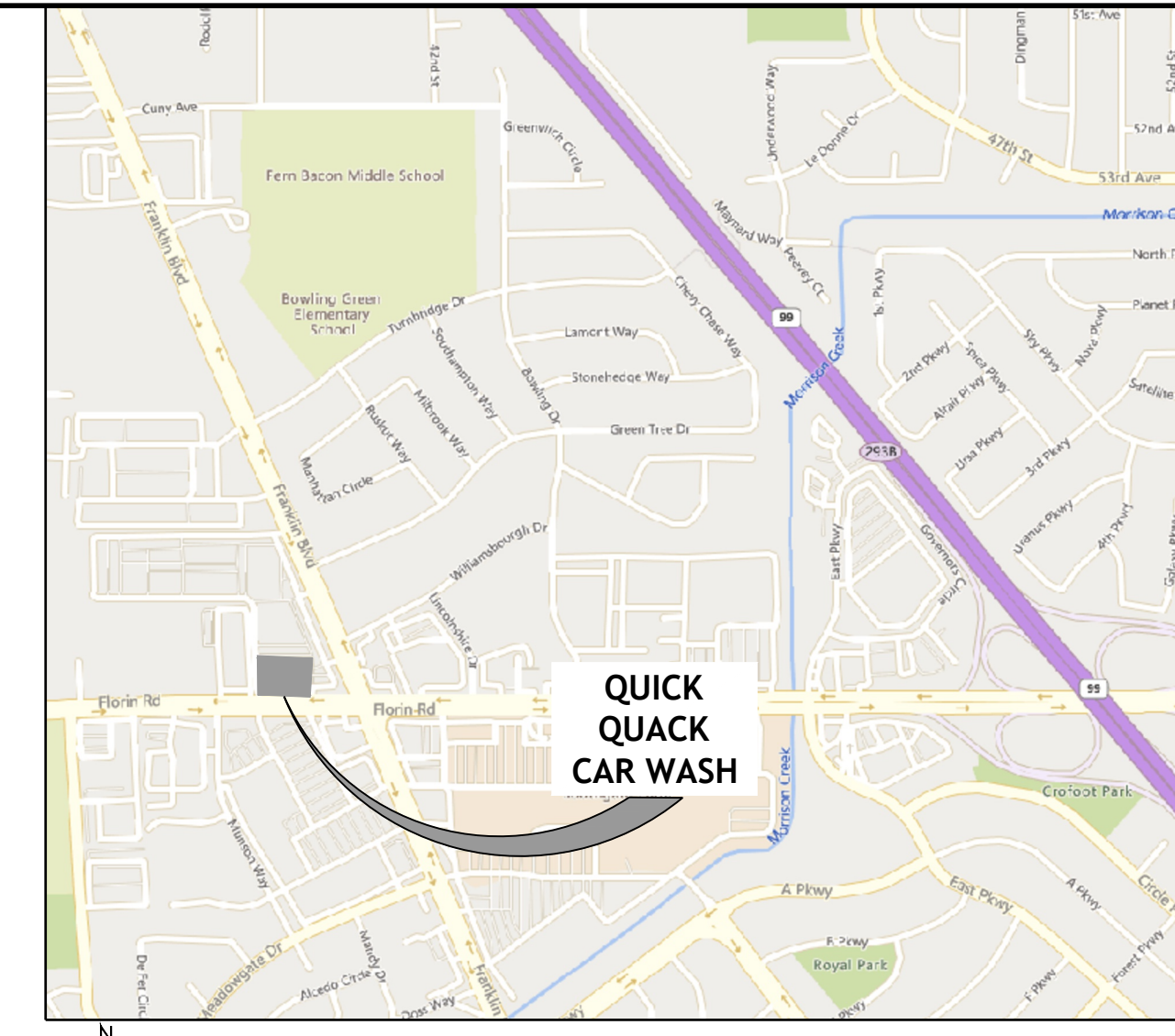
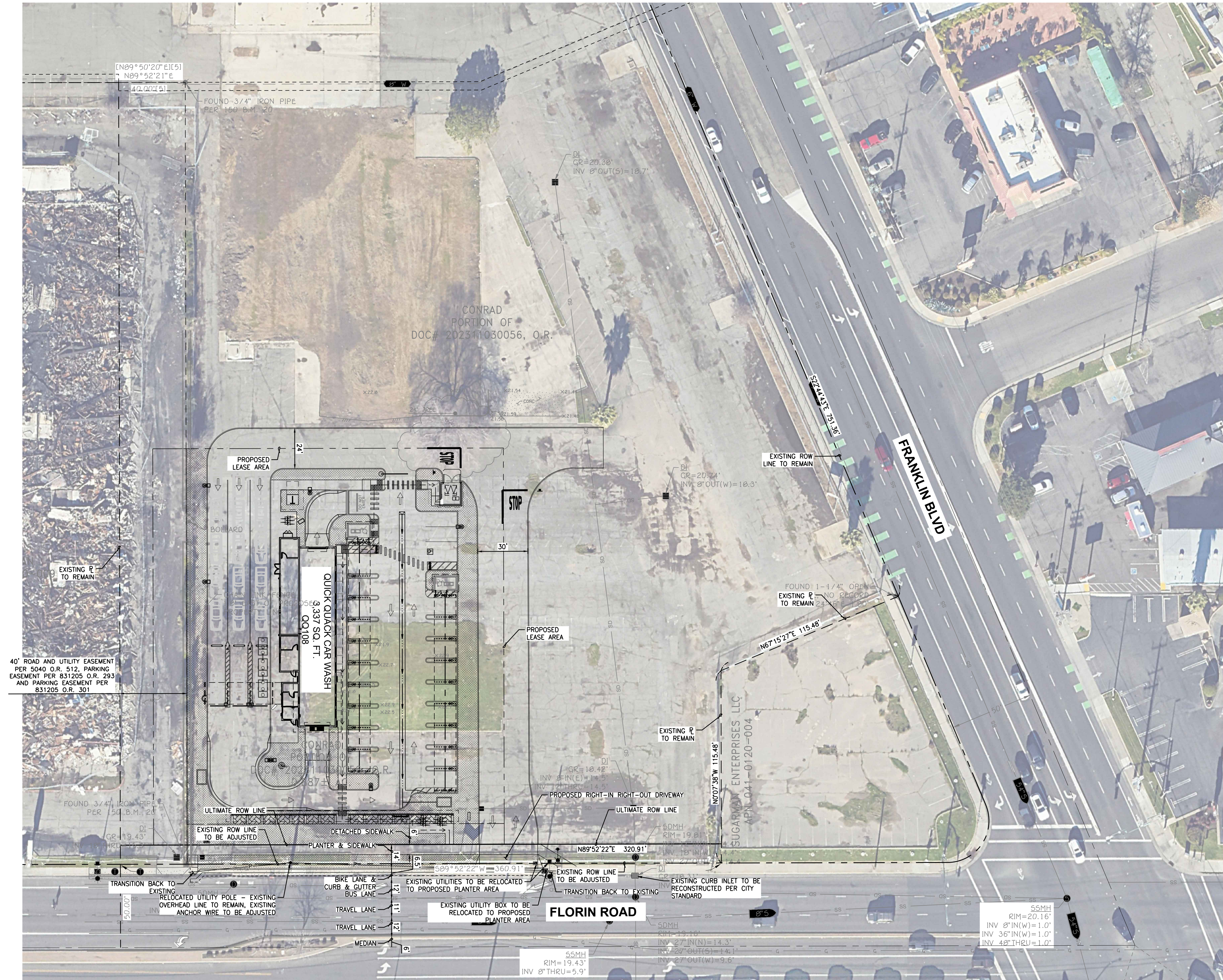
BARGHAUSEN CONSULTING ENGINEERS, INC  
18215 72ND AVENUE SOUTH  
KENT, WA 98032  
TEL: (425) 251-6222  
EMAIL: DGOALWIN@BARGHAUSEN.COM  
CONTACT: DAN GOALWIN

**LANDSCAPE ARCHITECT:**

OLIVE STREET LANDSCAPE ARCHITECTURE, INC.  
P.O. BOX 2083  
PETALUMA, CA 94952  
TEL: (707) 280-8990  
EMAIL: ROD@OLIVESTREETLANDSCAPE.COM  
CONTACT: RODNEY L. SCACALOSI

# OVERALL SITE PLAN FOR QUICK QUACK CAR WASH

AT THE SOUTHWEST QUARTER OF SECTION 32, TOWNSHIP 8  
NORTH, RANGE 5 EAST, IN SACRAMENTO COUNTY, CALIFORNIA.



**VICINITY MAP** SACRAMENTO, CA  
\*NOT TO SCALE

**LEGEND:**

PERVIOUS AREA

**ABBREVIATIONS:**

P PROPERTY LINE  
R RADIUS  
ROW RIGHT OF WAY

**PROJECT DATA**

LOCATION: 3815 FLORIN ROAD SACRAMENTO, CA 95823  
APN: 041-0120-022  
LOT SIZE: 620,712 SQ.FT./14.25 AC  
GROSS BUILDING AREA SQUARE FOOTAGE: 3,337 SF  
LOT COVERAGE: 9.9%  
ZONING: GENERAL COMMERCIAL (C-2)  
TOTAL LEASE AREA: 50,102 SQ. FT./1.15 AC  
TOTAL SITE DEVELOPMENT AREA: 61,634 SQ. FT.  
OCCUPANCY TYPE: B  
CONSTRUCTION TYPE: V-B  
PARKING DATA 1 STALLS PER 2,000 S.F. (OFFICE SPACE):  
- TOTAL PARKING REQUIRED: 2  
- TOTAL PARKING PROVIDED: 3 (2 STANDARD, 1 ADA)  
TOTAL NUMBER OF EMPLOYEES: 15  
MAX NUMBER OF EMPLOYEES WORKING AT A TIME: 2-3

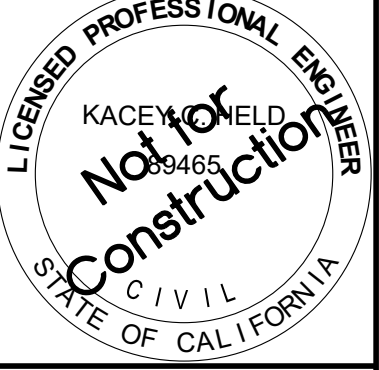
**EXISTING AREA SUMMARY:**

PROPOSED AREA OF DISTURBANCE FOR QUICK QUACK PROJECT: 61,634 SF (1.42 AC)  
EXISTING PERVIOUS AREA: 8,141 SF (0.19 AC)  
EXISTING IMPERVIOUS AREA: 53,493 SF (1.23 AC)  
PROPOSED PERVIOUS AREA: 11,314 SF (0.26 AC)  
PROPOSED IMPERVIOUS AREA: 50,320 SF (1.16 AC)

No.	Date	By	Chd.	Appr.	Revision

**OVERALL SITE PLAN**  
**QUICK QUACK CAR WASH NO. 26-643**  
**3815 FLORIN ROAD**  
**SACRAMENTO, CA 95823**

**For:**  
**QUICK QUACK CAR WASH**  
**6202 WEST OAKS BLVD. SUITE 300**  
**ROCKLIN, CA 95765**



Scale:	Horizontal	1"=30'	Vertical	N/A					
Designed	JLR	Drawn	ES	Checked	KCH	Approved	KCH	Date	7/16/25

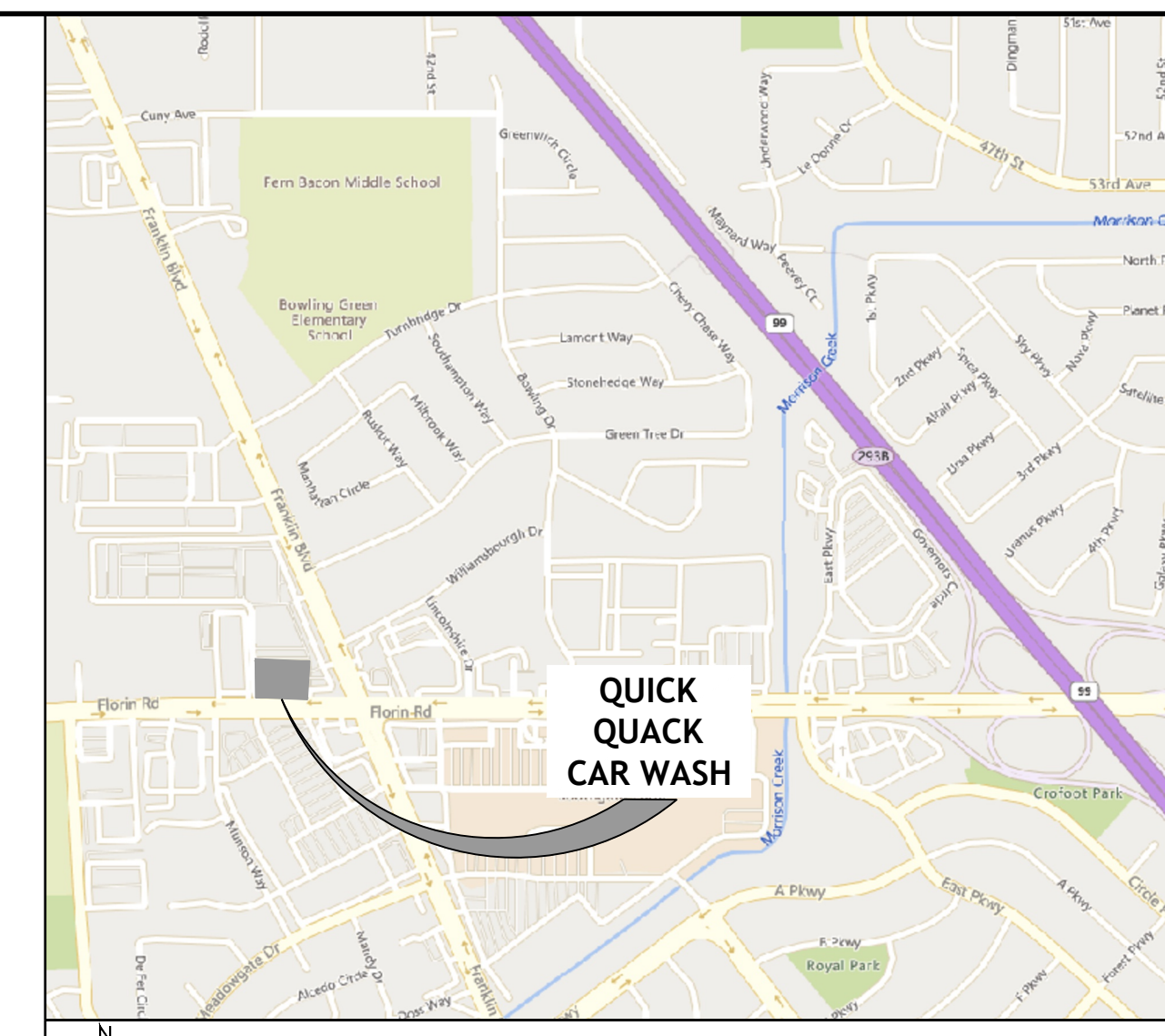
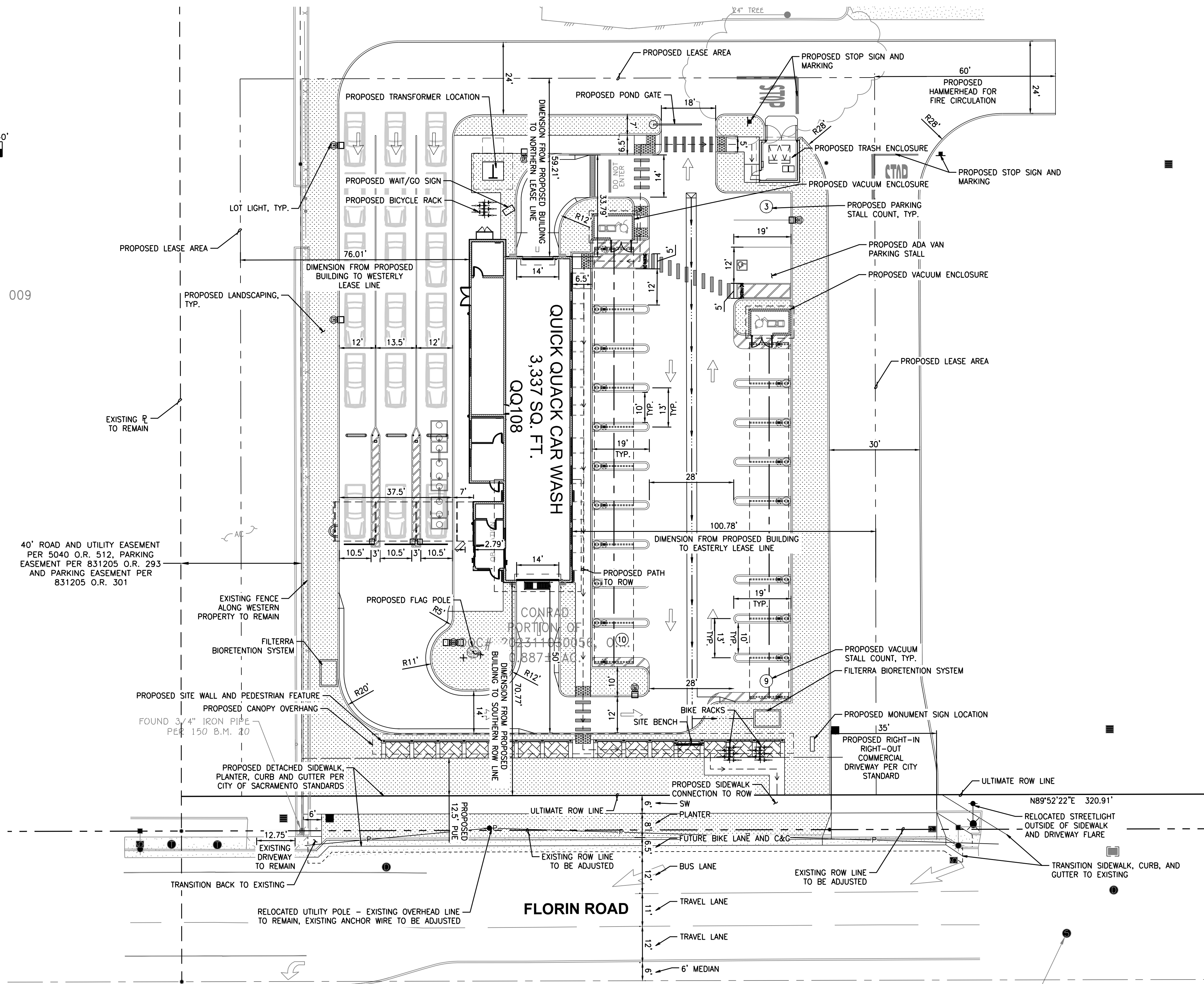
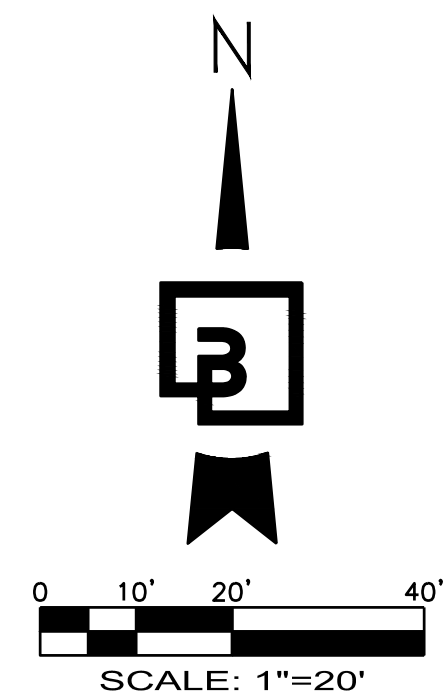
**Barghausen Consulting Engineers, LLC.**  
18215 72nd Avenue South  
Kent, WA 98032  
425.251.6222  
barghausen.com

Job Number  
**23559**  
Sheet  
**OSP**

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# PRELIMINARY SITE PLAN FOR QUICK QUACK CAR WASH

AT THE SOUTHWEST QUARTER OF SECTION 32, TOWNSHIP 8  
NORTH, RANGE 5 EAST, IN SACRAMENTO COUNTY, CALIFORNIA.



**VICINITY MAP** SACRAMENTO, CA  
\*NOT TO SCALE

### SITE INFORMATION

SITE NUMBER:	26-643
BUILDING:	QQ-108
SITE AREA:	1.15 ACRES
STACKING LANES:	3
STACKING WIDTH:	3 LANES @ 12' MIN
VACUUM STALLS:	19 STANDARD STALLS @ 10' X 19', 1 ADA STALL AT 17' X 19'
VACUUM PRODUCER:	2 UNITS @ 40 HP
ADA PARKING:	1 STALL @ 17' X 19'
BUILDING AREA:	3,337 SF
BUILDING SPRINKLED:	
QB STATION ATTACHED:	YES
BUILDING LENGTH:	108'-0"
SHOWROOM LENGTH:	
CONVEYOR TRENCH LENGTH:	
CONVEYOR LENGTH:	
CONVEYOR TYPE:	
SIGNATURE:	
APPROVAL DATE:	

### ABBREVIATIONS:

- P PROPERTY LINE
- R RADIUS
- ROW RIGHT OF WAY

### LEGEND:

- PERVIOUS AREA

### PROJECT DATA

LOCATION: 3815 FLORIN ROAD SACRAMENTO, CA 95823  
 APN: 041-0120-022  
 LOT SIZE: 620,712 SQ.FT./14.25 AC  
 GROSS BUILDING AREA SQUARE FOOTAGE: 3,337 SF  
 LOT COVERAGE: 9.9%  
 ZONING: GENERAL COMMERCIAL (C-2)  
 TOTAL LEASE AREA: 50,102 SQ. FT./1.15 AC  
 TOTAL SITE DEVELOPMENT AREA: 61,634 SQ. FT.  
 OCCUPANCY TYPE: B  
 CONSTRUCTION TYPE: V-B  
 PARKING DATA 1 STALLS PER 2,000 S.F. (OFFICE SPACE):  
 - TOTAL PARKING REQUIRED: 2  
 - TOTAL PARKING PROVIDED: 3 (2 STANDARD, 1 ADA)  
 TOTAL NUMBER OF EMPLOYEES: 15  
 MAX NUMBER OF EMPLOYEES WORKING AT A TIME: 2-3

### LANDSCAPE INFORMATION:

**MINIMUM REQUIRED:**  
 10% FOR PARKING AREAS AND DRIVE AISLES.  
 TOTAL LANDSCAPED AREA: 11,376 SF  
 (11,376/61,634) X 100% = 18.4% LANDSCAPED AREA PROVIDED

Revision  
No. Date By Ctd. Appr.

**PRELIMINARY SITE PLAN**  
**QUICK QUACK CAR WASH NO. 26-643**  
**3815 FLORIN ROAD**  
**SACRAMENTO, CA 95823**

**FOR:**  
**QUICK QUACK CAR WASH**  
**6202 WEST OAKS BLVD. SUITE 300**  
**ROCKLIN, CA 95765**



Scale:  
Horizontal 1"=20'  
Vertical N/A  
Designed: JLR  
Drawn: ES  
Checked: KCH  
Approved: KCH  
Date: 7/16/23

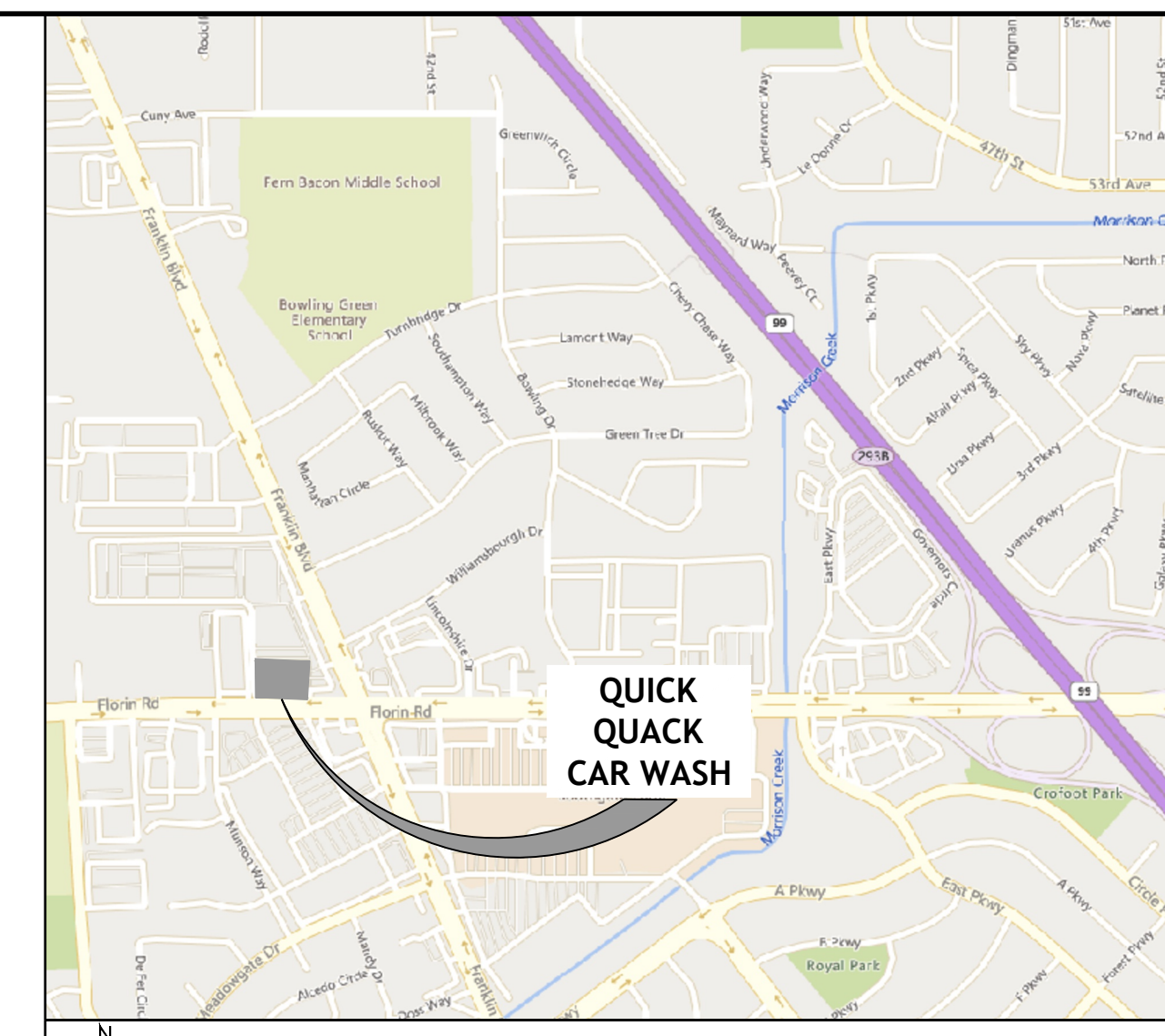
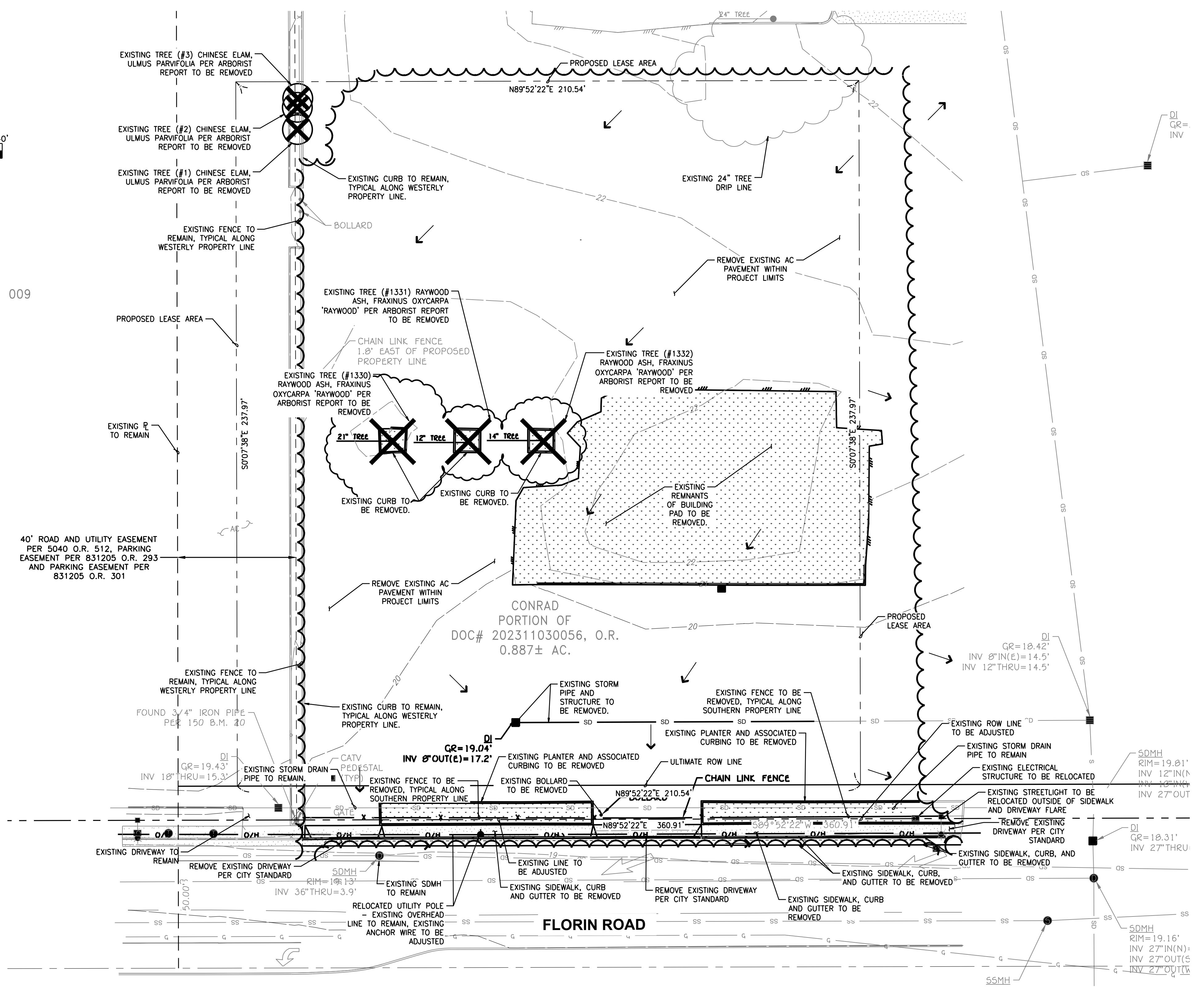
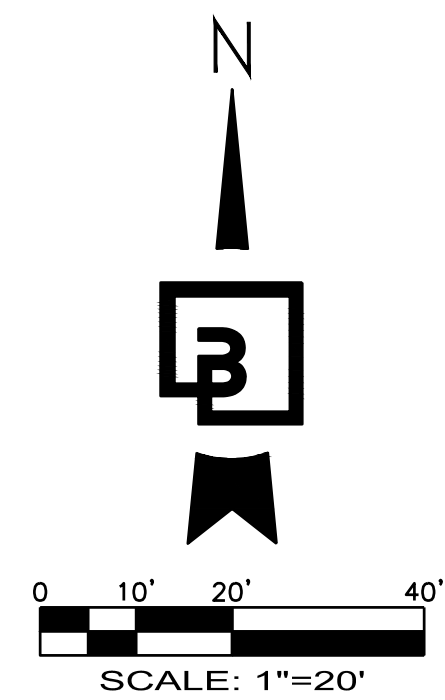
**Barghausen Consulting Engineers, LLC.**  
 18215 72nd Avenue South  
 Kent, WA 98032  
 425.251.6222  
 barghausen.com

Job Number  
**23559**  
Sheet  
**PSP**

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# PRELIMINARY DEMOLITION PLAN FOR QUICK QUACK CAR WASH

AT THE SOUTHWEST QUARTER OF SECTION 32, TOWNSHIP 8  
NORTH, RANGE 5 EAST, IN SACRAMENTO COUNTY, CALIFORNIA.



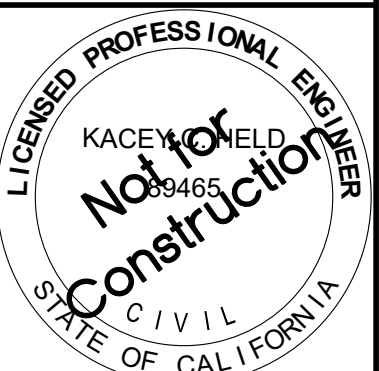
**VICINITY MAP** SACRAMENTO, CA  
\*NOT TO SCALE

- LEGEND:**
- LIMITS OF DISTURBANCE
  - PERVIOUS AREA
  - DRAINAGE PATH
- ABBREVIATIONS:**
- P PROPERTY LINE
  - R RADIUS
  - ROW RIGHT OF WAY
  - SDMH STORM DRAIN MANHOLE

No.	Date	By	Appr.	Revision

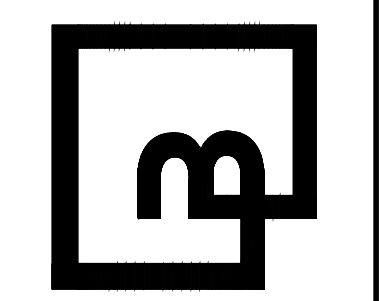
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**PRELIMINARY DEMOLITION PLAN**  
**QUICK QUACK CAR WASH NO. 26-643**  
**3815 FLORIN ROAD**  
**SACRAMENTO, CA 95823**

**For:**  
**QUICK QUACK CAR WASH**  
**6202 WEST OAKS BLVD. SUITE 300**  
**ROCKLIN, CA 95765**



Scale:	Horizontal	Vertical
	1"=20'	N/A
Designed:	JLR	ES
Drawn:	ES	KCH
Checked:	KCH	KCH
Approved:	KCH	N/A
Date:	7/16/25	

**Barghausen Consulting Engineers, LLC.**  
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[barghausen.com](http://barghausen.com)



**Job Number**  
**23559**

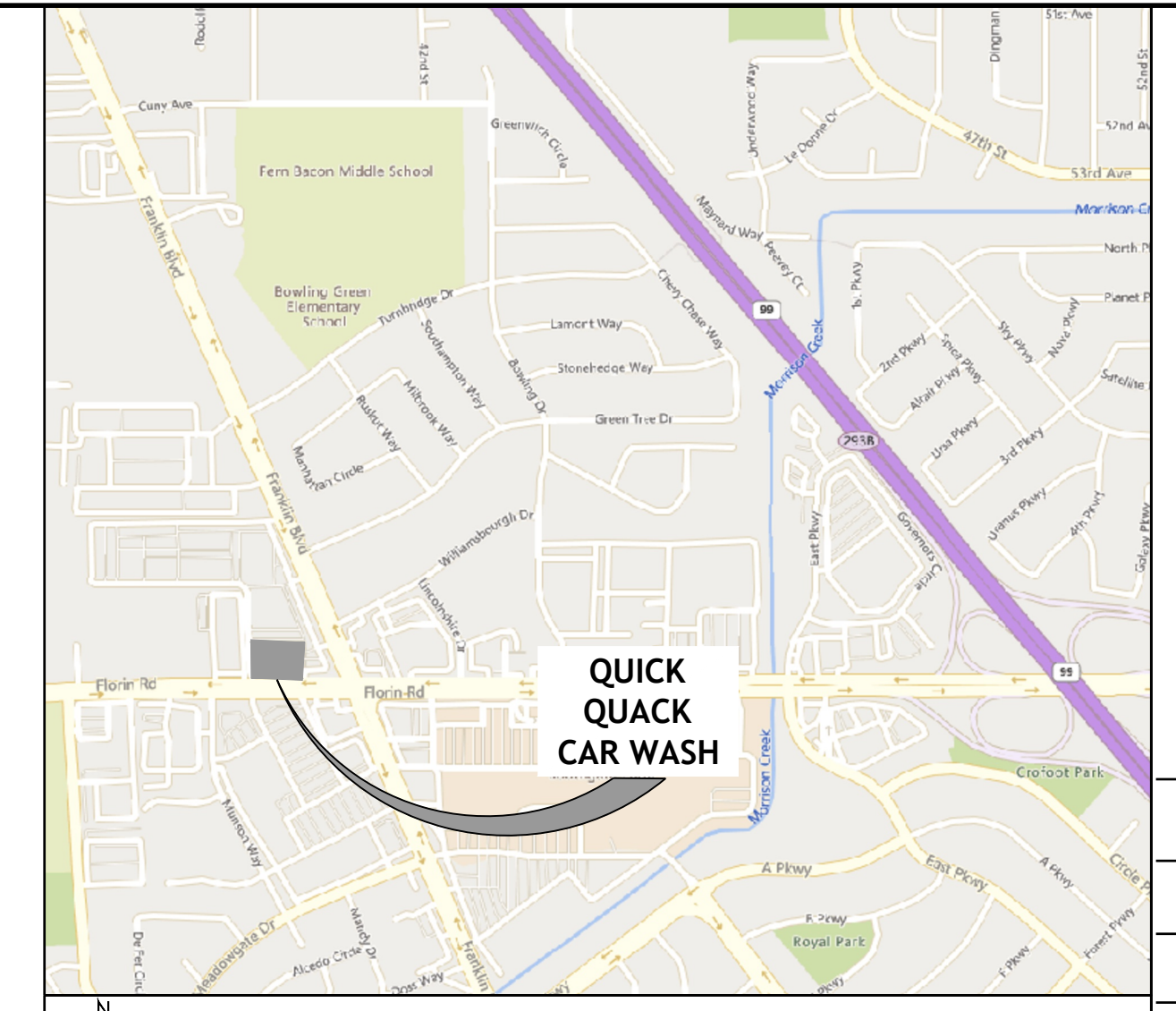
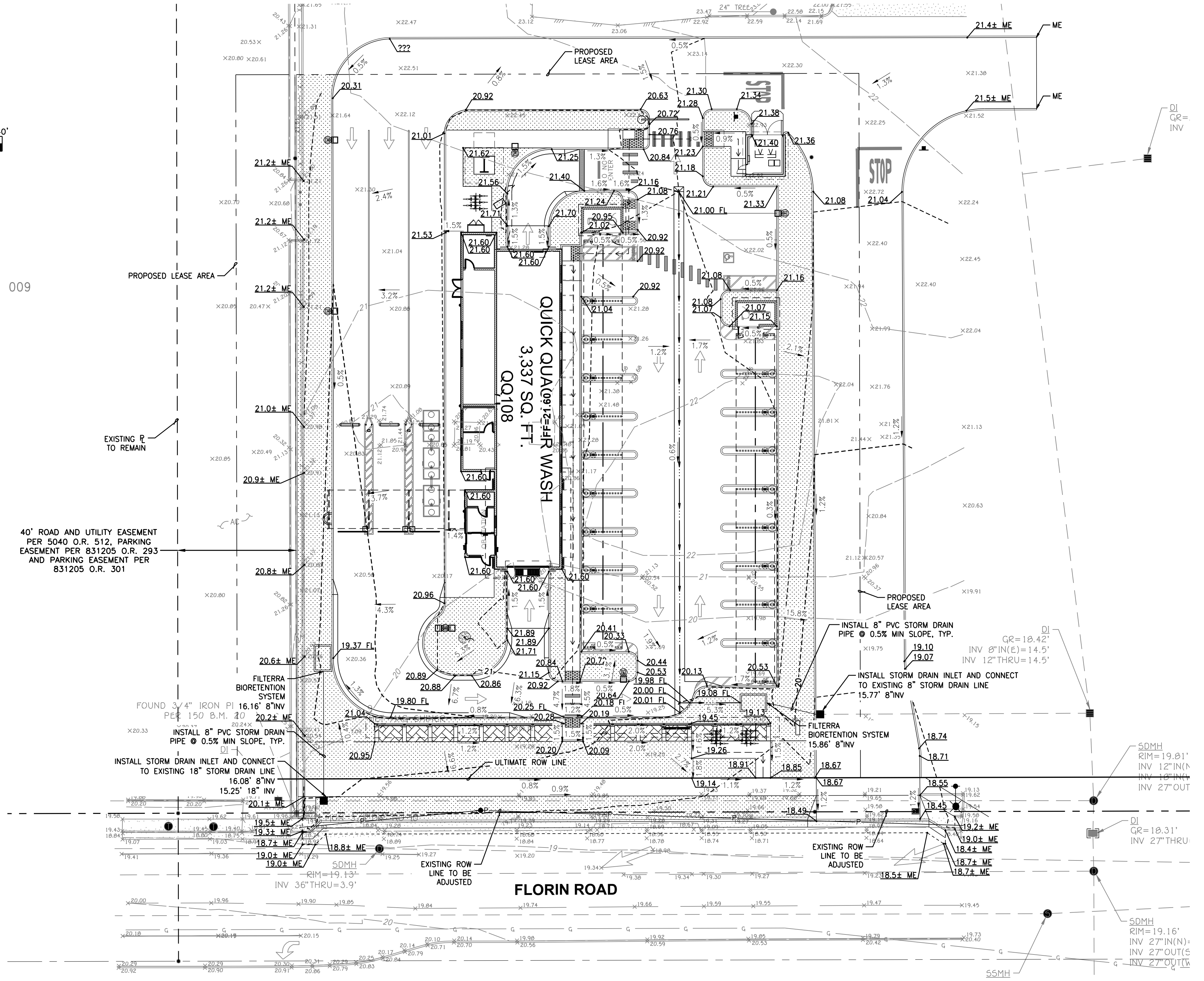
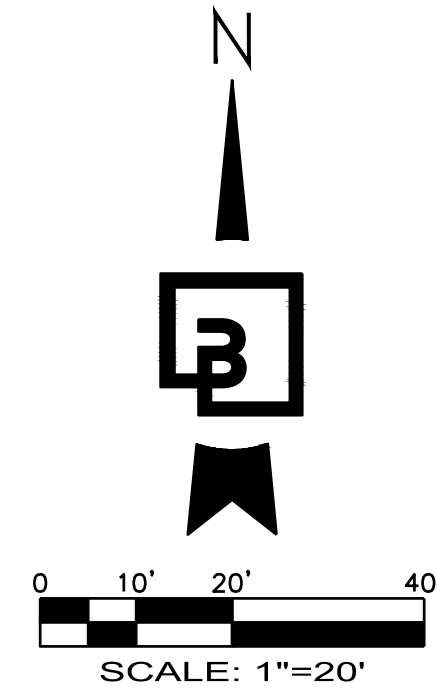
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PRELIMINARY GRADING AND DRAINAGE PLAN

FOR  
**QUICK QUACK CAR WASH**

AT THE SOUTHWEST QUARTER OF SECTION 32, TOWNSHIP 8  
NORTH, RANGE 5 EAST, IN SACRAMENTO COUNTY, CALIFORNIA.



**VICINITY MAP** SACRAMENTO, CA  
\*NOT TO SCALE

**GRADING NOTES:**

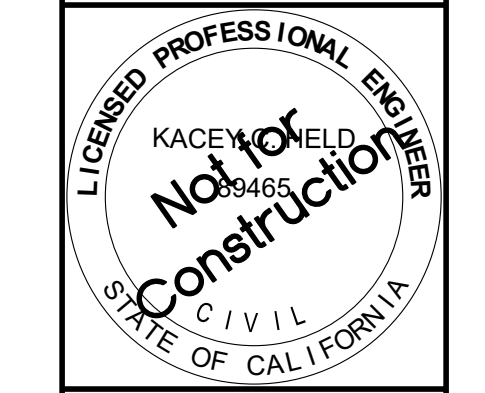
1. ALL GRADING SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE PROVISIONS OF THE CURRENT CITY ORDINANCE AND STANDARD PLANS. THE GRADING IS SUBJECT TO THE OBSERVATION AND APPROVAL OF THE ENGINEERING DIVISION.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR VERIFYING ALL PROPOSED GRADES IN RELATIONSHIP TO SURVEYED BASIS OF ELEVATION.
3. ALL EXISTING UTILITY STRUCTURES AND ASSOCIATED LIDS THAT FALL WITHIN THE AREA OF WORK SHALL BE ADJUSTED TO FINISHED GRADE ELEVATIONS. CONTRACTOR SHALL CONFIRM THE FEASIBILITY OF ADJUSTING EXISTING UTILITY STRUCTURE LIDS TO FINISHED GRADE PRIOR TO CONSTRUCTION AND NOTIFY ENGINEER OF ANY DISCREPANCIES.
4. CONTRACTOR TO PROTECT AND MAINTAIN EROSION CONTROL FACILITIES DURING GRADING OPERATIONS.
5. CONTRACTOR TO OBTAIN ANY NECESSARY RIGHT-OF-WAY PERMITS IF REQUIRED FOR WORK SHOWN ON PLANS.
6. CONTRACTOR SHALL MATCH EXISTING ELEVATIONS AT ALL SAWCUT LOCATIONS.
7. ALL SPOT ELEVATIONS SHOWN ARE TO FINISH SURFACE (TOP OF ASPHALT OR TOP OF CONCRETE PAVEMENT, NOT TOP OF CURB/SIDEWALK) UNLESS OTHERWISE NOTED.

**LEGEND:**

PROPOSED CONTOUR	---
EXISTING CONTOUR	---
GRADE BREAK/RIDGELINE	---
PROPOSED GRADING SLOPE	1.00%
SPOT ELEVATION	80.40
MATCH EXISTING ELEVATION	80.40± ME
FLOW LINE	80.40 FL

Revision  
No. Date By Ctd. Appr.  
Title:  
**PRELIMINARY GRADING AND DRAINAGE PLAN**  
**QUICK QUACK CAR WASH NO. 26-643**  
**3815 FLORIN ROAD**  
**SACRAMENTO, CA 95823**

For:  
**QUICK QUACK CAR WASH**  
**6202 WEST OAKS BLVD, SUITE 300**  
**ROCKLIN, CA 95765**



Scale:	Horizontal	1"=20'	Vertical	N/A
Designed JLR	Draw ES	Checked KCH	Approved KCH	Date 7/16/23

**Barghausen Consulting Engineers, LLC.**  
18215 72nd Avenue South  
Kent, WA 98032  
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Job Number  
**23559**  
Sheet  
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**PLANT SCHEDULE**

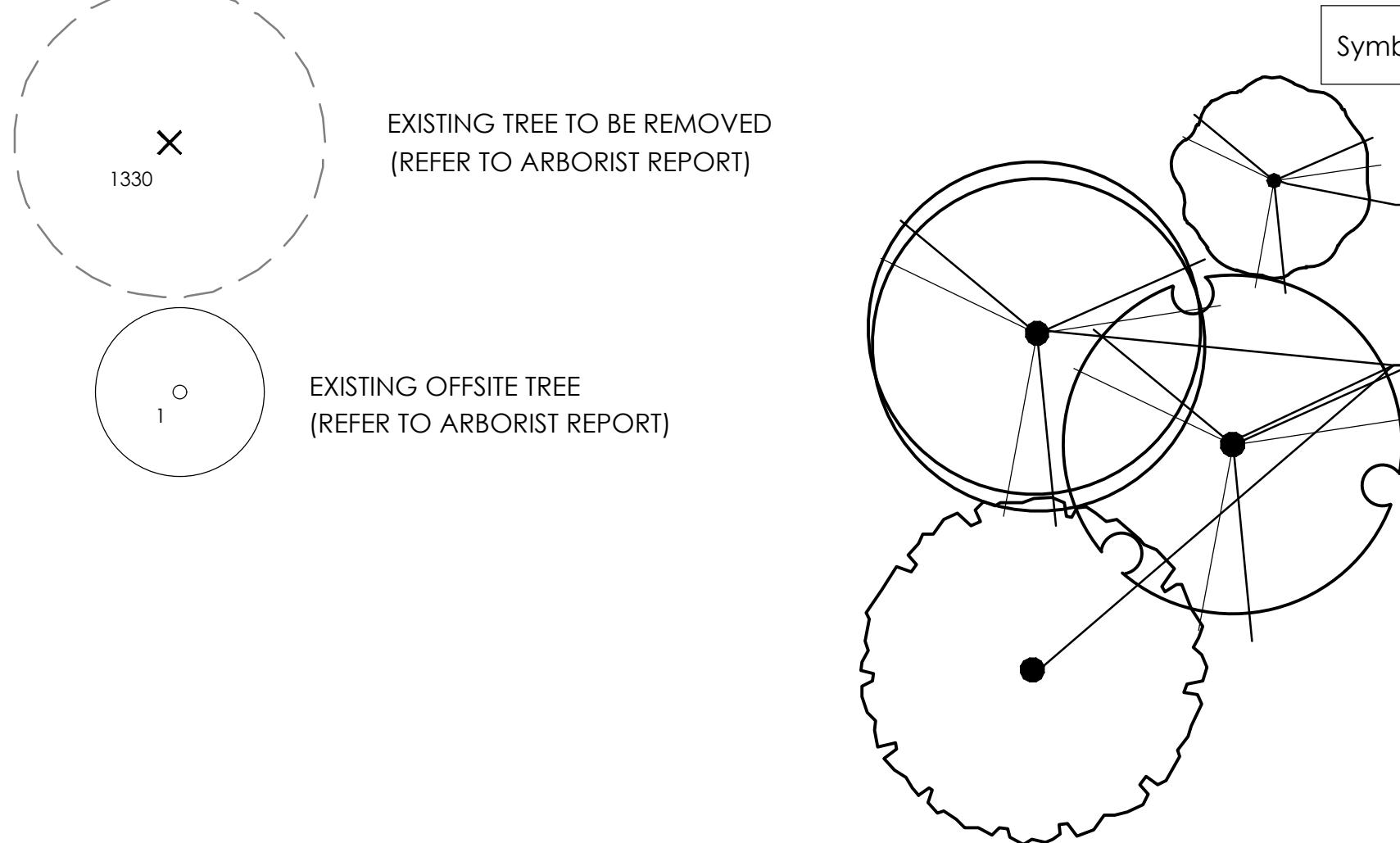
SYMBOL	QTY	BOTANICAL / COMMON NAME	CONT	WUCOLS
<b>TREES</b>				
	2	Acer rubrum 'October Glory' / October Glory Red Maple	24" box	Moderate
	6	Cercis occidentalis / Western Redbud Standard form	24" box	Low
	3	Lagerstroemia indica 'Red' / Red Crape Myrtle	15 gal.	Low
	6	Quercus suber / Cork Oak	24" box	Low
	5	Zelkova serrata 'Village Green' / Village Green Japanese Zelkova	24" box	Moderate

SYMBOL	QTY	BOTANICAL / COMMON NAME	CONT	WUCOLS
<b>SHRUBS</b>				
	146	Callistemon viminalis 'Little John' / Little John Weeping Bottlebrush	5 gal.	Low
	135	Dianella revoluta 'DR5000' / Little Rey™ Flax Lily	1 gal.	Low
	142	Lomandra longifolia 'Lomlon' / Lime Tuff Dwarf Mat Rush	1 gal.	Low
	12	Muhlenbergia rigens / Deer Grass	5 gal.	Low
	24	Olea europaea 'Montra' / Little Olive® Olive	5 gal.	Low
	40	Phormium tenax 'Dark Delight' / Dark Delight Flax	5 gal.	Low
	53	Rhaphiolepis umbellata 'Minor' / Dwarf Yedda Hawthorn	5 gal.	Low
	29	Salvia greggii 'Furmans Red' / Furman's Red Autumn Sage	1 gal.	Low
	23	Westringia fruticosa 'WES05' / Mundit™ Coast Rosemary	5 gal.	Low
	57	Yucca x 'Bright Star' / Variegated Spanish Dagger	5 gal.	Low

SYMBOL	QTY	BOTANICAL / COMMON NAME	CONT	WUCOLS
<b>VINES/ESPALLIERS</b>				
	8	Macfadyena unguis-cati / Cat's Claw Creeper	5 gal.	Low

SYMBOL	QTY	BOTANICAL / COMMON NAME	CONT	WUCOLS	SPACING
<b>GROUND COVERS</b>					
	95	Grevillea lanigera 'Coastal Gem' / Coastal Gem Grevillea	1 gal.	Low	60" o.c.
	55	Rosa x 'Meisentmil' / Lemon Drift® Rose	1 gal.	Low	30" o.c.
	40	Teucrium chamaedrys / Germander	1 gal.	Low	30" o.c.

**EXISTING PLANT LEGEND**



**GENERAL NOTES**

- All landscape areas shall receive a minimum 3" layer of organic wood chip mulch, natural color. Gorilla hair (also known as shredded mulch) shall not be used.
- Refer to Preliminary Arborist Report by Focal Point Arboriculture consulting for existing onsite and offsite trees shown on this plan. Reference number as shown next to existing trees on this plan are referenced as such in the report.
- Landscape planting and irrigation shall conform to City of Sacramento Water Conservation Standards.
- Install linear root barriers for all trees located within 7" of pavement or a structure. See detail on sheet L3.
- Property owner shall agree that all new trees, including Crape Myrtles, shall not be 'head' pruned or topped any time after installation.
- A certified soils analysis report and recommendations will be conducted after site rough grading has been completed. A copy of the report must be presented to the City of Sacramento Planning prior to planting operations. Contractor shall follow recommendations of soils analysis report.
- Contact USA North (811) prior to commencing construction in order to determine any conflicts with existing utilities.
- Certificate of Completion shall be included on the plans and shall contain the following six (6) elements: (15.92.130)
  - Project information sheet that contains:
    - Date
    - Project name
    - Project applicant name, telephone and mailing address
    - Project address and location
    - Property owner name, telephone and mailing address
  - Certification by either the signer of the landscape design plan or licensed landscape contractor who installed the landscaping and irrigation:
    - Where significant changes are made in the field during construction "as-built" or record drawings shall be included.
    - A copy of the irrigation plan or record drawing shall be placed with the irrigation controller
  - Irrigation scheduling parameters used to set the controller. (15.92.140)
  - Landscape and irrigation maintenance schedule (15.92.150)
  - Irrigation audit report (15.92.160)
  - Soil analysis report, if not submitted with landscape document package (15.92.090)

**Water Efficient Landscape Worksheet**

HYDROZONE/PLANTING DESCRIPTION	PLANT FACTOR (PF)	IRRIG. METHOD	IRRIG. EFFICIENCY	ETAF (PF/IE)	AREA (Sq. Ft.)	ETAF x AREA	ESTIMATED TOTAL WATER USE
<b>REGULAR LANDSCAPE AREA</b>							
LOW WATER USE	0.3	DRIP	0.81	0.37	10,179	3,770	121311
MODERATE WATER USE	0.5	DRIP	0.81	0.62	400	247	7945
<b>TOTALS</b>					10,579	4,017	129256

**ETAF Calculations**

Total ETAF x Area	4,017
Total Area	10,579
Average ETAF	38%

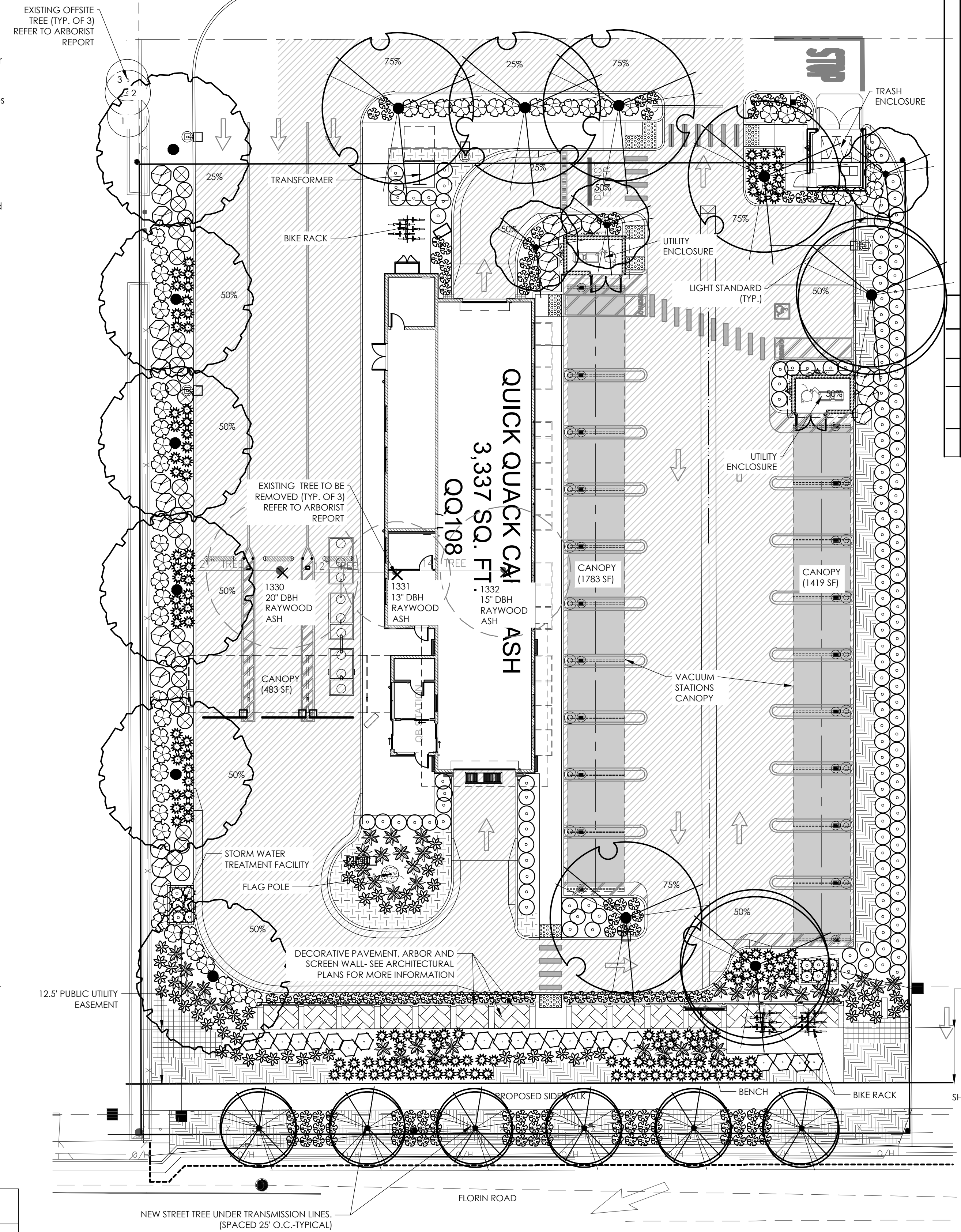
Note:  
1. Average ETAF for Regular Landscape areas must be below .55 for residential areas and .45 for non-residential areas.

Maximum Applied Water Allowance (MAWA). MAWA = (ETAF) (Conversion factor) ((ETAF)(Landscape Area)) + ((1-ETAF) x SLA). (51.9) (.62) (.45 x 10579) + (1-.45) x 0 = 159185

Estimated Total Water use (ETWU). ETWU = (ETAF) (Conversion factor) ((ETAF) (Area)). (51.9) (.62) (4017) = 129256

**PARKING LOT SHADE CALCULATIONS**

Symbol	Botanical Name/ Common Name	Qty. @ full shade (Sq. Ft.)	Qty. @ 3/4 Shade (Sq. Ft.)	Qty. @ 1/2 shade (Sq. Ft.)	Qty. @ 1/4 shade (Sq. Ft.)	Total (Sq. Ft.)
	Canopy structures	NA	NA	NA	NA	3,685 SF
	Parking Lot Shade Tree 20'	0 @ 314 SF	0 @ 236 SF	2 @ 157 SF	0 @ 79 SF	314 SF
	Parking Lot Shade Tree 30'	0 @ 706 SF	0 @ 530 SF	0 @ 354 SF	0 @ 177 SF	0 SF
	Parking Lot Shade Tree 35'	0 @ 962 SF	4 @ 722 SF	7 @ 481 SF	3 @ 240 SF	7,937 SF
<b>TOTAL TREE SHADE</b>						11,936 SF
Paved Area (see hatched area on plan)						TOTAL PAVED AREA = 21,772 SF
						50% SHADE AREA REQUIRED = 10,886 SF
						TOTAL SHADE PROVIDED = 11,936 SF
						PERCENT SHADE = 55%



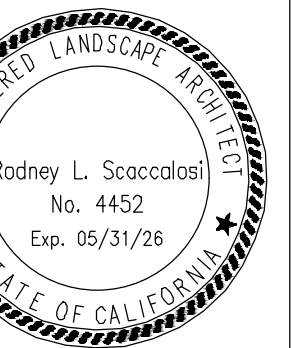
**COMPLIANCE STATEMENT**

I have complied with the criteria of the Sacramento City Code Chapter 15.92 and applied them accordingly for the efficient use of water in the landscape design plan.

*Podney Deccaron* 10-27-25

**PRELIMINARY LANDSCAPE PLAN**  
QUICK QUACK CAR WASH NO. 26-643  
3815 FLORIN ROAD  
SACRAMENTO, CA 95823

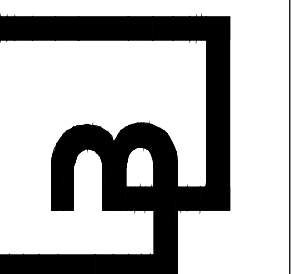
**QUICK QUACK CAR WASH, SUITE 300**  
6202 WEST OAKS BLVD.  
ROCKLIN, CA 95765



Scale:  
Horizontal 1" = 16'  
Vertical N/A

Designed: xxx  
Drawn: xxx  
Checked: xxx  
Approved: xxx  
Date: 10-27-25

**Barghausen Consulting Engineers, Inc.**  
18215 72nd Avenue South  
Kent, WA 98032  
425.251.6222  
barghausen.com



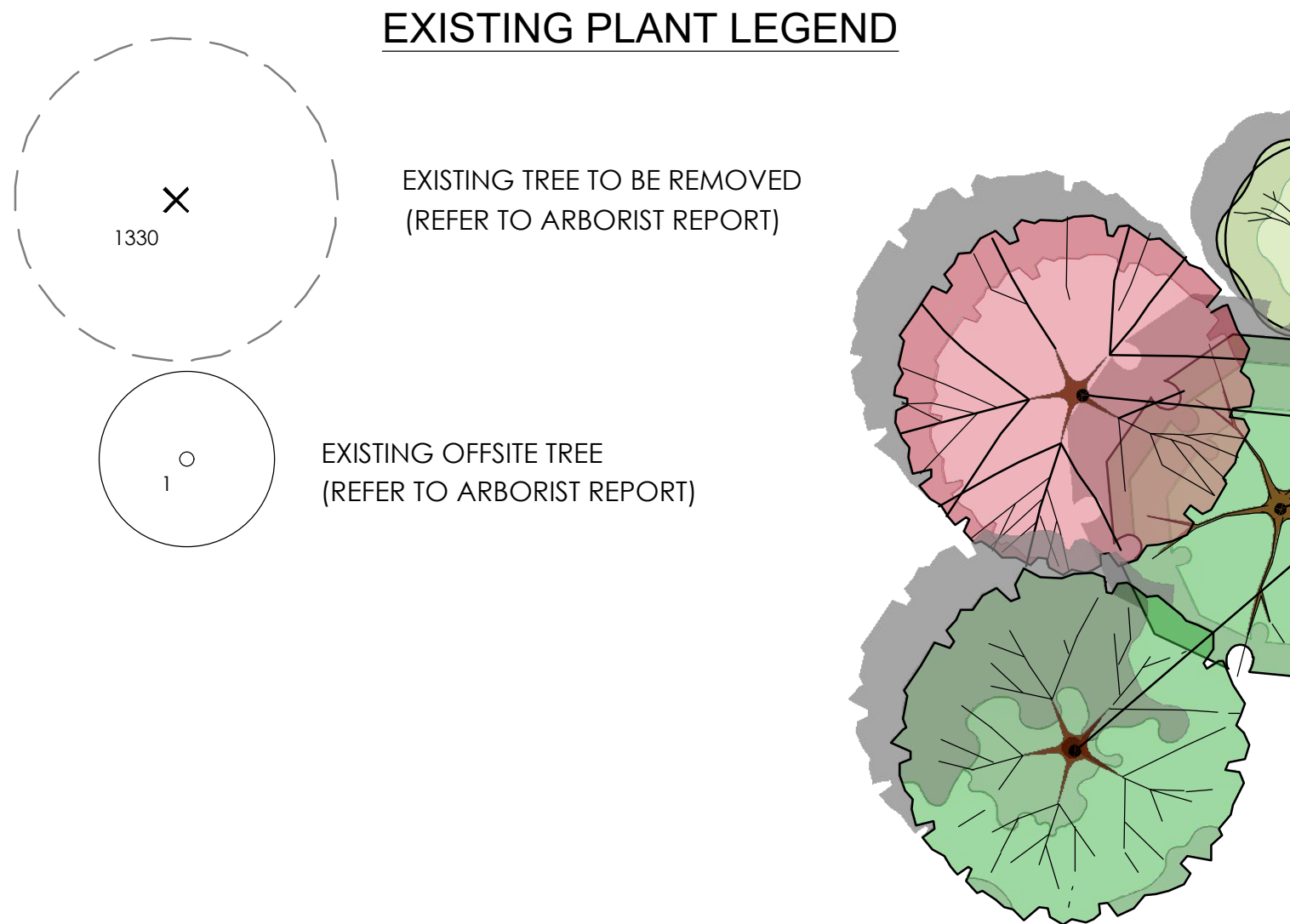
Job Number: 23559  
Sheet: L1

**OLIVE STREET**  
LANDSCAPE ARCHITECTURE  
P.O. Box 2083  
Petaluma CA 94952  
707-280-8990  
OliveStreetLandscape.com  
rod@olivestreetlandscape.com

**PLANT SCHEDULE**

SYMBOL	QTY	BOTANICAL / COMMON NAME	CONT	WUCOLS
<b>TREES</b>				
	2	Acer rubrum 'October Glory' / October Glory Red Maple	24" box	Moderate
	6	Cercis occidentalis / Western Redbud Standard form	24" box	Low
	3	Lagerstroemia indica 'Red' / Red Crape Myrtle	15 gal.	Low
	6	Quercus suber / Cork Oak	24" box	Low
	5	Zelkova serrata 'Village Green' / Village Green Japanese Zelkova	24" box	Moderate
<b>SHRUBS</b>				
	146	Callistemon viminalis 'Little John' / Little John Weeping Bottlebrush	5 gal.	Low
	135	Dianella revoluta 'DR5000' / Little Rey™ Flax Lily	1 gal.	Low
	142	Lomandra longifolia 'Lomlon' / Lime Tuff Dwarf Mat Rush	1 gal.	Low
	12	Muhlenbergia rigens / Deer Grass	5 gal.	Low
	24	Olea europaea 'Montra' / Little Ollie® Olive	5 gal.	Low
	40	Phormium tenax 'Dark Delight' / Dark Delight Flax	5 gal.	Low
	53	Rhaphiolepis umbellata 'Minor' / Dwarf Yedda Hawthorn	5 gal.	Low
	29	Salvia greggii 'Furman's Red' / Furman's Red Autumn Sage	1 gal.	Low
	23	Westringia fruticosa 'WES05' / Mundit™ Coast Rosemary	5 gal.	Low
	57	Yucca x 'Bright Star' / Variegated Spanish Dagger	5 gal.	Low
<b>VINES/ESPALLIERS</b>				
	8	Macfadyena unguis-cati / Cat's Claw Creeper	5 gal.	Low

SYMBOL	QTY	BOTANICAL / COMMON NAME	CONT	WUCOLS	SPACING
<b>GROUND COVERS</b>					
	95	Grevillea lanigera 'Coastal Gem' / Coastal Gem Grevillea	1 gal.	Low	60" o.c.
	55	Rosa x 'Meisentmil' / Lemon Drift® Rose	1 gal.	Low	30" o.c.
	40	Teucrium chamaedrys / Germander	1 gal.	Low	30" o.c.



**GENERAL NOTES**

- All landscape areas shall receive a minimum 3" layer of organic wood chip mulch, natural color. Gorilla hair (also known as shredded mulch) shall not be used.
- Refer to Preliminary Arborist Report by Focal Point Arboriculture consulting for existing onsite and offsite trees shown on this plan. Reference number as shown next to existing trees on this plan are referenced as such in the report.
- Landscape planting and irrigation shall conform to City of Sacramento Water Conservation Standards.
- Install linear root barriers for all trees located within 7" of pavement or a structure. See detail on sheet L3.
- Property owner shall agree that all new trees, including Crape Myrtles, shall not be 'head' pruned or topped any time after installation.
- A certified soils analysis report and recommendations will be conducted after site rough grading has been completed. A copy of the report must be presented to the City of Sacramento Planning prior to planting operations. Contractor shall follow recommendations of soils analysis report.
- Contact USA North (811) prior to commencing construction in order to determine any conflicts with existing utilities.
- Certificate of Completion shall be included on the plans and shall contain the following six (6) elements: (15.92.130)
  - Project information sheet that contains:
    - Date
    - Project name
    - Project applicant name, telephone and mailing address
    - Project address and location
    - Property owner name, telephone and mailing address
  - Certification by either the signer of the landscape design plan or licensed landscape contractor who installed the landscaping and irrigation:
    - Where significant changes are made in the field during construction "as-built" or record drawings shall be included.
    - A copy of the irrigation plan or record drawing shall be placed with the irrigation controller
  - Irrigation scheduling parameters used to set the controller. (15.92.140)
  - Landscape and irrigation maintenance schedule (15.92.150)
  - Irrigation audit report (15.92.160)
  - Soil analysis report, if not submitted with landscape document package (15.92.090)

**Water Efficient Landscape Worksheet**

HYDROZONE/PLANTING DESCRIPTION	PLANT FACTOR (PF)	IRRIG. METHOD	IRRIG. EFFICIENCY	ETAF (PF/IE)	AREA (Sq. Ft.)	ETAF x AREA	ESTIMATED TOTAL WATER USE
<b>REGULAR LANDSCAPE AREA</b>							
LOW WATER USE	0.3	DRIP	0.81	0.37	10,179	3,770	121311
MODERATE WATER USE	0.5	DRIP	0.81	0.62	400	247	7945
<b>TOTALS</b>					10,579	4,017	129256

**ETAF Calculations**

Total ETAF x Area	4,017
Total Area	10,579
Average ETAF	38%

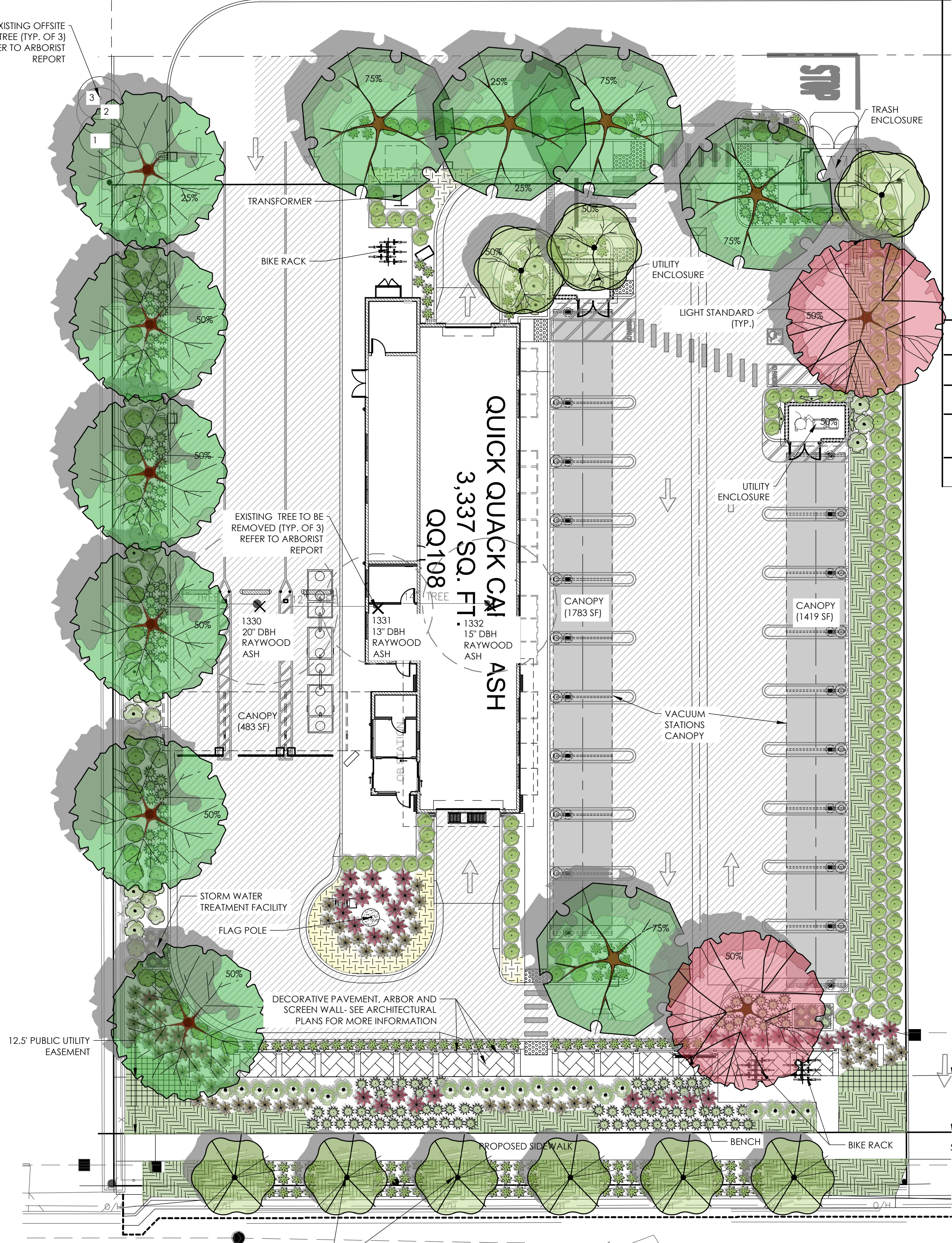
Note:  
 1. Average ETAF for Regular Landscape areas must be below .55 for residential areas and .45 for non-residential areas.

Maximum Applied Water Allowance (MAWA). MAWA = (ETAF) (Conversion factor) ((ETAF)(Landscape Area)) + ((1-ETAF) x SLA).  
 (51.9) (.42) (.45 x 10579) + (1-.45) x 0 = 159185

Estimated Total Water Use (ETWU). ETWU = (ETAF) (Conversion factor) ((ETAF) (Area)).  
 (51.9) (.42) (4017) = 129256

**PARKING LOT SHADE CALCULATIONS**

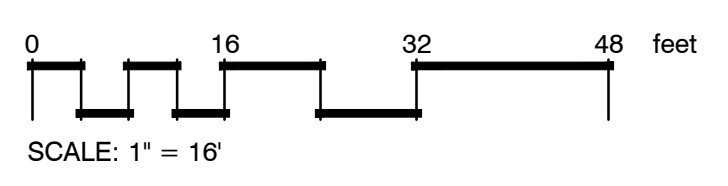
Symbol	Botanical Name/ Common Name	Qty. @ full shade (Sq. Ft.)	Qty. @ 3/4 Shade (Sq. Ft.)	Qty. @ 1/2 shade (Sq. Ft.)	Qty. @ 1/4 shade (Sq. Ft.)	Total (Sq. Ft.)
	Canopy structures	NA	NA	NA	NA	3,685 SF
	Parking Lot Shade Tree 20'	0 @ 314 SF	0 @ 236 SF	2 @ 157 SF	0 @ 79 SF	314 SF
	Parking Lot Shade Tree 30'	0 @ 706 SF	0 @ 530 SF	0 @ 354 SF	0 @ 177 SF	0 SF
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**COMPLIANCE STATEMENT**

I have complied with the criteria of the Sacramento City Code Chapter 15.92 and applied them accordingly for the efficient use of water in the landscape design plan.

*Podney Deccaron* 10-27-25



Revision

No.	Date	By	Check	Appr.

Title: **PRELIMINARY LANDSCAPE PLAN**  
**QUICK QUACK CAR WASH NO. 26-643**  
**3815 FLORIN ROAD**  
**SACRAMENTO, CA 95823**

For: **QUICK QUACK CAR WASH, SUITE 300**  
**6202 WEST OAKS BLVD. SUITE 300**  
**ROCKLIN, CA 95765**

Scale: Horizontal 1" = 16' Vertical N/A

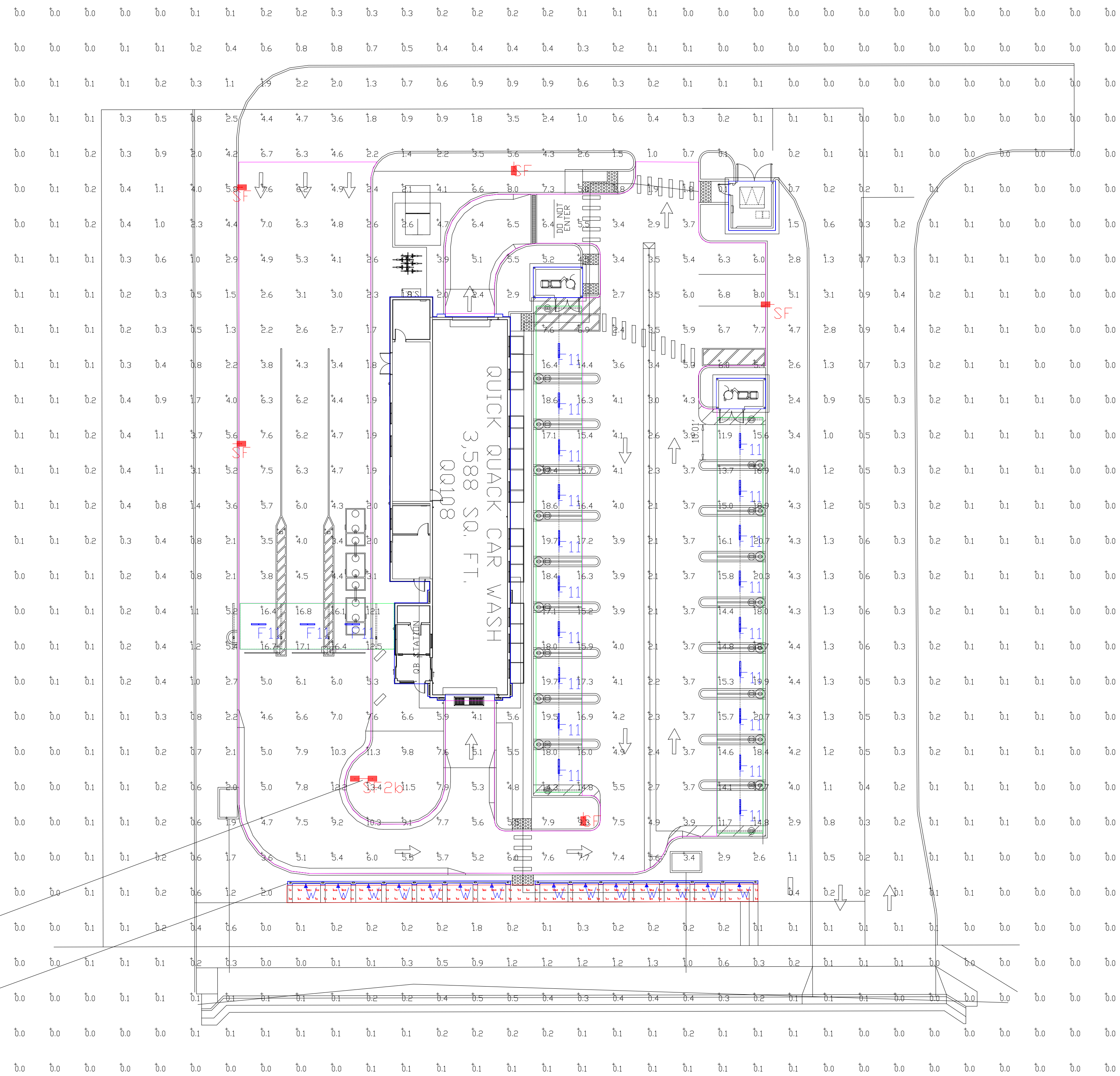
Designed: xxx  
 Drawn: xxx  
 Checked: xxx  
 Approved: xxx  
 Date: 2-11-26

**Barghausen Consulting Engineers, Inc.**  
 18215 72nd Avenue South  
 Kent, WA 98032  
 425.251.6222  
 barghausen.com

**OLIVE STREET LANDSCAPE ARCHITECTURE**  
 P.O. Box 2083  
 Petaluma CA 94952  
 707-280-8990  
 OliveStreetLandscape.com  
 rod@olivestreetlandscape.com

Job Number: **23559**  
 Sheet: **L1**

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POLE TO BE USED FOR FLAG POLE LIGHT

8 1/2' WALL



Note: For national account pricing/quotation and order placement, please reach out to Steve Friedman at Hermitage Lighting - 847-830-1444 or sfriedman@gohermitage.com.

NOTE: 4SQ-XX-S11G-16FT POLE + 2FT BASE  
POLE ORDERING INFORMATION BASED ON THIS DRAWING;  
PLEASE CONFIRM EPA WIND RATING PRIOR TO ORDER.  
CUSTOMER RESPONSIBLE FOR BASE SELECTION PER LOCAL CONDITIONS.

PHOTOMETRIC EVALUATION  
NOT FOR CONSTRUCTION

Based on the information provided, all dimensions and luminaire locations shown represent recommended positions. The engineer and/or architect must determine the applicability of the layout to existing or future field conditions.

This lighting plan represents illumination levels calculated from laboratory data taken under controlled conditions in accordance with The Illuminating Engineering Society (IES) approved methods. Actual performance of any manufacturer's luminaires may vary due to changes in electrical voltage, tolerance in lamps/LED's and other variable field conditions. Calculations do not include obstructions such as buildings, curbs, landscaping, or any other architectural elements unless noted. Fixture nomenclature noted does not include mounting hardware or poles. This drawing is for photometric evaluation purposes only and should not be used as a construction document or as a final document for ordering product.

The IES no longer uses the Cutoff Classification System for LED fixtures. The IES classifies LED fixtures with the BUG rating which refers to the Backlight-Uplight-Glare system. An Uplight of "00" most closely matches the old Full Cutoff rating.

Label	CalcType	Units	Avg	Max	Min	Avg/Min	Max/Min	Grid Z
ALL CALC POINTS AT GRADE	Illuminance	Fc	2.43	20.7	0.0	N.A.	N.A.	0
WALKWAY	Illuminance	Fc	22.75	122.1	0.2	113.75	610.50	0
PAVED AREA	Illuminance	Fc	4.67	12.2	1.7	2.75	7.18	0
PAY CANDPY	Illuminance	Fc	15.51	17.1	12.1	1.28	1.41	0
VACUUM CANDPY 1	Illuminance	Fc	16.25	19.7	6.9	2.36	2.86	0
VACUUM CANDPY 2	Illuminance	Fc	16.40	20.7	11.7	1.40	1.77	0

Symbol	Qty	Label	Arrangement	Description	Mounting Height	LLF	Arr. Lum. Lumens	Arr. Watts	BUG Rating
	22	F11	Single	VT4454SUNVS8-46W-5000K (FIXTURE SUPPLIED BY HERMITAGE)	10' 6"	1.000	6470	44.5	B2-U3-G2
	5	SF	SINGLE	MRS-LED-18L-SIL-FT-50-70CRI-SINGLE	16' POLE + 2' BASE	1.000	16890	135	B2-U0-G2
	1	SF2b	D180°	MRS-LED-18L-SIL-FT-50-70CRI-D180	16' POLE + 2' BASE	1.000	33780	270	B2-U0-G2
	14	W	Single	XWS-LED-01L-SIL-4-40-70CRI	1'4"	1.000	1529	10	B0-U0-G1

Dimensions of drawings that have been scaled or converted from PDF files or scanned /submitted images are approximate.

Total Project Watts  
Total Watts = 2064

QUICK QUACK #26-643  
3815 FLORIN RD  
SACRAMENTO, CA

DATE: 7/24/24 REV: 7/15/25 SHEET 1 OF 1

SCALE: 1"=20'















**# KEYNOTES DESCRIPTION**

- |   |  |
|---|--|
| <p>1 CONVEYOR TRENCH. SLOPE AT 1/4" PER FOOT TO DRAIN.<br/>                 2 UTILITY TRENCH WITH GRATE. SLOPE BOTTOM TO DRAIN.<br/>                 3 3'-4" WIDE X 2'-2" DEEP OPEN FOR ACCESS TO UTILITY TRENCH, 1X1 FIBERGLASS GRATE. CONTRACTOR TO PROVIDE COVER - MATCH UTILITY TRENCH<br/>                 4 TRENCH DRAIN<br/>                 5 ROOF ACCESS LADDER.<br/>                 6 EMPLOYEE LOCKERS. (TOTAL OF 3 - 1 ADA ACCESSIBLE). CONTRACTOR SUPPLIED AND INSTALLED.<br/>                 7 WALL MOUNTED ENCLOSED FIRE EXTINGUISHER. - 2A-10BC. (TYPICAL)<br/>                 8 ROOF DRAIN LEADER (TYPICAL). SEE PLUMBING PLANS.<br/>                 9 ELECTRICAL SERVICE SWITCHGEAR. SEE ELECTRICAL DRAWINGS.<br/>                 10 CUT RECESS IN SLAB FOR TIRE SENSOR SWITCH. (TYPICAL) QQ TO CONFIRM LOCATION.<br/>                 11 HOSE BIB IN RECESS BOX. SEE PLUMBING DRAWINGS.<br/>                 12 4" CONCRETE FILLED PIPE BOLLARD AT ENTRANCE.<br/>                 13 OUTLINE OF ROOF ABOVE.<br/>                 14 3'-0" X 3'-0" RECLAIM CLEAN-OUT. SEE STRUCTURAL DRAWINGS.<br/>                 15 EMERGENCY STOP BUTTONS. LOCATE PER QUICK QUACK REPRESENTATIVE. SEE ELECTRICAL DRAWINGS FOR LOCATIONS.<br/>                 16 WALL MOUNTED IT CABINET. CONTRACTOR TO PROVIDE BLOCKING. SEE ELECTRICAL DRAWINGS.<br/>                 17 KNOX BOX - CONTRACTOR TO COORDINATE WITH LOCAL FIRE DEPARTMENT FOR TYPE, SIZE AND PREFERRED LOCATION.<br/>                 18 4" FLOOR DRAIN SEE PLUMBING.</p> | <p>19 4" DRAIN PIPE STUBBED UP 12" A.F.F., PAINT GREEN. SEE PLUMBING DRAWINGS.<br/>                 20 CONTRACTOR FURNISHED AND INSTALLED 20'X54'X96" TALL STORAGE CABINET WITH ADJUSTABLE SHELVING AND WHITE MELAMINE FINISH ON THE INSIDE AND GRAY OUTSIDE.<br/>                 21 LEVEL LANDING PER CBC 404.2.3 AND TABLE 404.2.3.2 (TYP) - 24" MIN STRIKE EDGE CLEARANCE FOR THE EXTERIOR DOOR LANDING.<br/>                 22 EMPLOYEE LOUNGE REFRIDGERATOR.<br/>                 23 CONTRACTOR SUPPLIED AND INSTALLED EYE WASH STATION. SEE EQUIPMENT PLAN SHEET A2.14 AND PLUMBING DRAWINGS.<br/>                 24 34" MAXIMUM A.F.F. COUNTER TOP. CONTRACTOR TO PROVIDE BLOCKING.<br/>                 25 6" SWEEPS FROM UTILITY TRENCH TO EQUIPMENT ROOM VERIFY WITH OWNER FOR EXACT LOCATION. (TYPICAL)<br/>                 26 CONTRACTOR FURNISHED AND INSTALLED 18'X36'X40" TALL POS CABINET.<br/>                 27 3/4" X 2 1/2" WIDE RECESS IN SLAB FOR DRAINAGE EXTEND PAST EXIT OPENING 1'-0" MINIMUM.<br/>                 28 NEED CHASES TO EACH VACUUM ENCLOSURE. LONG SWEEPS ONLY. GC TO INSTALL AND PULL 3/4" PEX LINE FROM EQUIPMENT ROOM TO ENCLOSURES WOUND UP TO 10' COIL ON EACH END. SCHEDULE 40 SWEEPS.<br/>                 29 32" WIDE X 16" HIGH AIR VENT. SEE EXTERIOR ELEVATIONS.<br/>                 30 WALL MOUNTED CANOPY ABOVE. (TYPICAL)<br/>                 31 RECLAIM VENT PIPES.</p> |
|---|--|

**GENERAL NOTES**

- REFER TO WALL LEGEND FOR PARTITION TYPES.
- THE APPROVED AND STAMPED CONSTRUCTION DOCUMENTS SHALL REMAIN ON SITE AT ALL TIMES
- IT IS THE INTENT THAT THIS PROJECT IS IN COMPLIANCE WITH ALL FEDERAL, STATE, AND LOCAL CODES AND REGULATIONS. THE GENERAL CONTRACTOR SHALL NOTIFY THE ARCHITECT IMMEDIATELY UPON THE DISCOVERY OF ANY DISCREPANCIES BETWEEN THE CONSTRUCTION DOCUMENTS. ANY WORK IN QUESTION SHALL NOT COMMENCE UNTIL WRITTEN CLARIFICATION IS ISSUED BY THE ARCHITECT.
- UNLESS NOTED OTHERWISE, MASONRY CONTROL JOINTS SHALL BE LOCATED SUCH THAT NO STRAIGHT RUN OF MASONRY WALL EXCEED 24'-0". REFER TO STRUCTURAL DWG'S.
- EXIT DOORS SHALL BE OPERABLE FROM THE INSIDE WITHOUT THE USE OF A KEY OR ANY SPECIAL KNOWLEDGE.
- GC TO COORDINATE ALL REQUIRED BLOCKING FOR WALL HUNG EQUIPMENT, SHELVES, LIGHTING, ETC. FOR PROPER INSTALLATION HEIGHTS.
- TACTILE EXITS SIGNS SHALL BE REQUIRED AT THE FOLLOWING LOCATIONS:
  - WHEREVER BASIC CBC PROVISIONS REQUIRE EXIT SIGNS FROM A ROOM OR AREA TO CORRIDOR OR HALLWAY. THE TACTILE SIGN SHALL HAVE THE WORDS, "EXIT ROUTE."
  - EACH GRADE-LEVEL EXIT DOOR. THE TACTILE EXIT SIGN SHALL HAVE THE WORD, "EXIT."

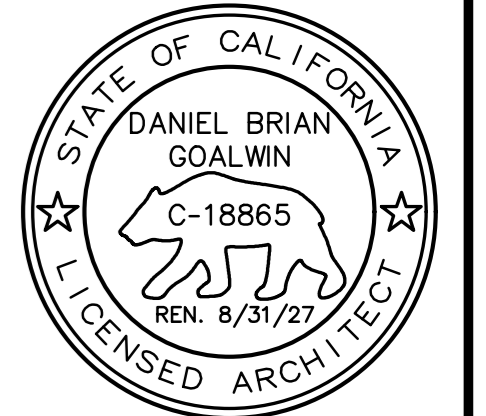
**DIMENSIONS NOTES:**

- IN NO CASE SHALL WORKING DIMENSIONS BE SCALED FROM PLANS, SECTIONS OR DETAILS ON DRAWINGS.
- ALL DIMENSIONS TO OPENINGS ARE TO FACE OF MASONRY UNLESS OTHERWISE NOTED.
- ALL DIMENSIONS TO STUD WALL PARTITIONS ARE TO THE FACE OF STUD UNLESS OTHERWISE NOTED.
- GRID LINES ARE TO FACE OF MASONRY.
- FINISH FLOOR ELEVATIONS NOTED ARE REFERENCED FROM DATUM POINT 0'-0" AT EQUIPMENT ROOM FINISH FLOOR AT DOOR THRESHOLD.
- ALL DIMENSIONS SHALL BE VERIFIED IN THE FIELD PRIOR TO FABRICATION OF ANY INSTALLATION. DIMENSIONS ARE NOT ADJUSTABLE WITHOUT APPROVAL OF ARCHITECT IN WRITING.
- ALL DIMENSIONS SHALL BE VERIFIED IN THE FIELD PRIOR TO FABRICATION OF ANY INSTALLATION. DIMENSIONS ARE NOT ADJUSTABLE WITHOUT APPROVAL OF ARCHITECT IN WRITING.
- MINIMUM AND MAXIMUM DIMENSIONS AND SLOPES AS REQUIRED FOR DISABLED ACCESS ARE TO TAKE PRECEDENCE IN ALL CASES OF CONFLICT. CODE REQUIRED MINIMUMS AND MAXIMUM ARE TO BE EXCEEDED WHERE POSSIBLE AND WHERE NOTED. NOTIFY ARCHITECT OF ANY DIMENSIONAL DISCREPANCIES PRIOR TO CONSTRUCTION.
- DIMENSIONS AND GRAPHIC REPRESENTATIONS OCCURRING ON LARGER DRAWINGS SHALL TAKE PRECEDENCE OVER SMALLER SCALE DRAWINGS.

SYMBOL	DESCRIPTION
	CMU WALL CONSTRUCTION
	METAL STUD WALL @ 16" OC
	6" METAL STUD FRAMING @ 16" OC
	WALL PARTITION INDICATOR
	DETAIL INDICATOR
	DOOR SYMBOL
	WINDOW SYMBOL
	ELEVATION SYMBOLS
	WINDOW
	FIRE EXTINGUISHER: 2A-10B-C; 5 LBS; MODEL # 8500 WITH WALL BRACKET 888-16591 BY AMEREX OR APPROVED EQUAL - 75" MAX. TRAVEL DISTANCE TO EXTINGUISHER (CFC 906) - FINAL APPROVAL/LOCATION AS DETERMINED BY FIRE DEPT. INSPECTOR.
	TACTILE EXIT SIGN PER CBC.

www.barghausen.com

SEAL:

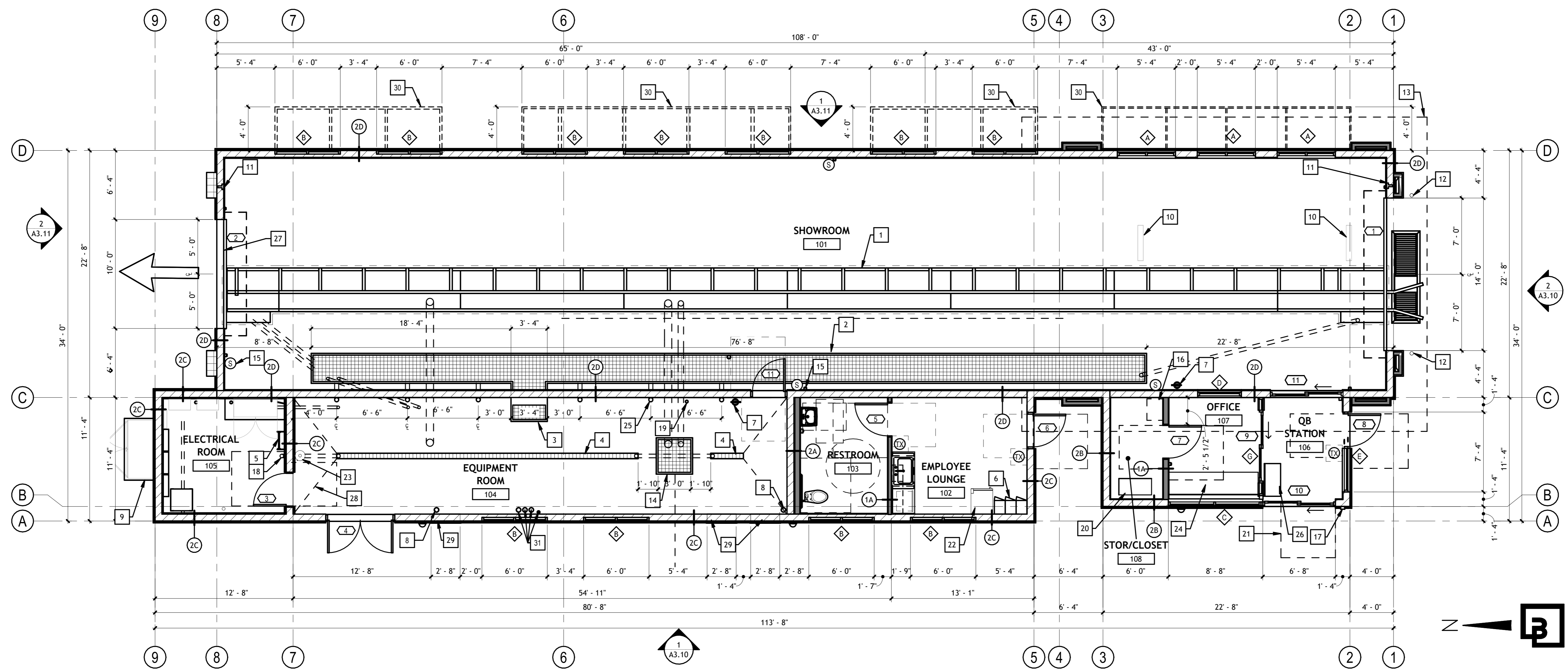


CONSULTANT:  
 Consultant:

**KEYNOTES - FLOOR PLAN**

**GENERAL NOTES**

**SYMBOL LEGEND**



**QUICK QUACK CAR WASH 26-643**



**3815 FLORIN ROAD  
 SACRAMENTO, CA 95823**

REV	DATE	DESCRIPTION

DRAWN BY: MB  
 CHECKED BY: MU  
 ARCH. PROJECT NO.: 23559  
 SHEET NAME: FLOOR PLAN  
 SHEET NUMBER: A2.11

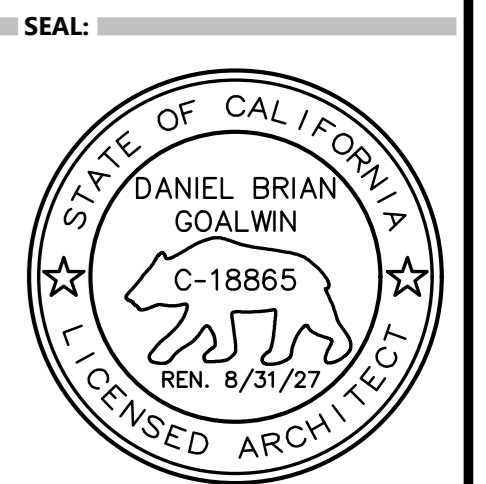
**FLOOR PLAN**

3/16" = 1'-0"

**A2.11**







CONSULTANT:  
Consultant:

QUICK QUACK CAR WASH 26-643

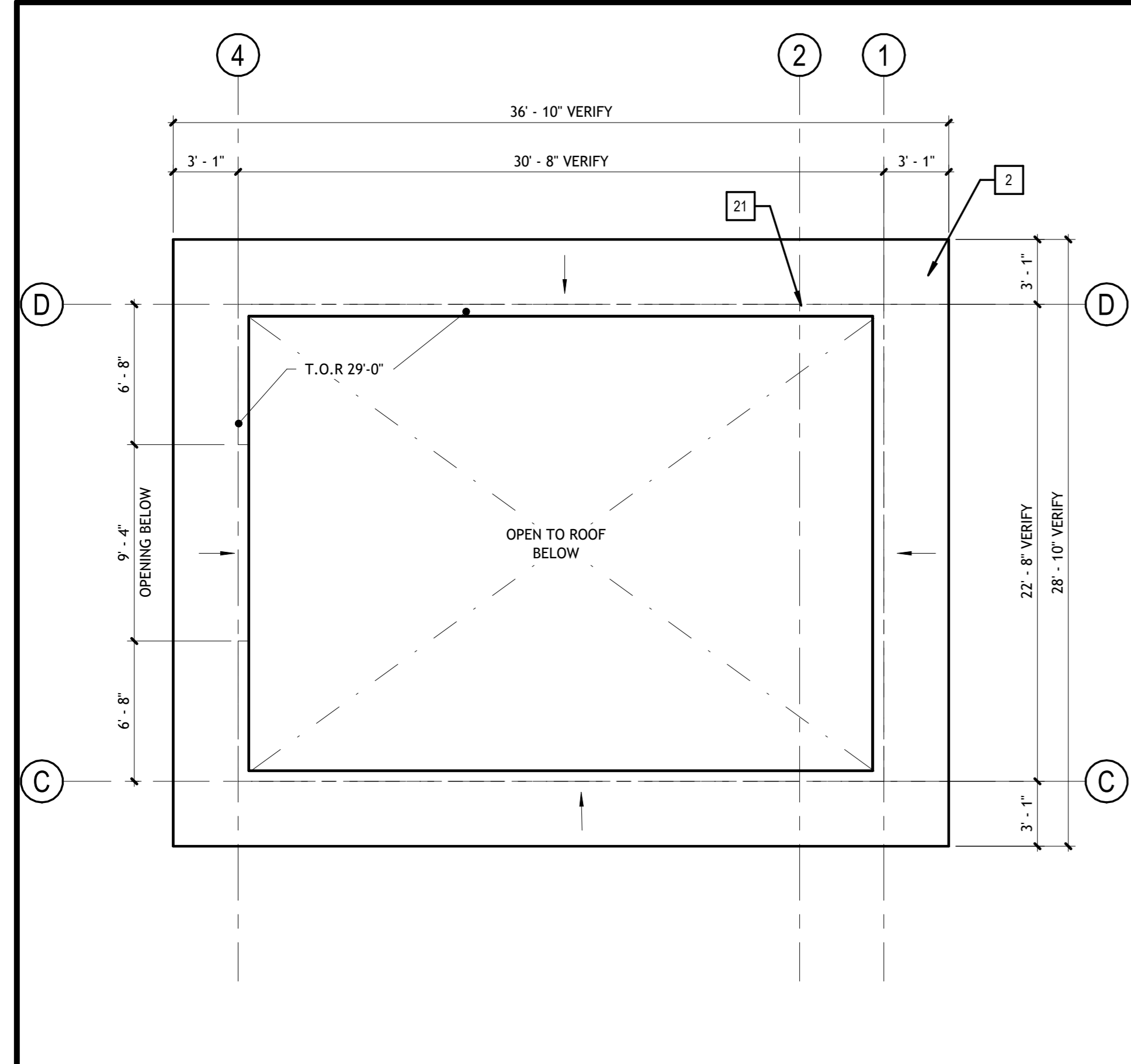


3815 FLORIN ROAD  
SACRAMENTO, CA 95823

REV	DATE	DESCRIPTION

DRAWN BY: MB  
CHECKED BY: MU  
ARCH. PROJECT NO.: 23559  
SHEET NAME:

ROOF PLAN  
SHEET NUMBER:  
**A2.30**



UPPER ROOF PLAN 3/16" = 1'-0" 2

- # KEYNOTES DESCRIPTION
- 1 TPO ROOFING MEMBRANE AND ROOFING ASSEMBLY. SEE TPO ROOFING NOTES ON SHEET A8.10 (ASTM E 108, CLASS A RATED).
  - 2 PRE-FINISHED 18 GA., MTL. ROOF.
  - 3 PRE-FINISHED METAL COPING AT TOP OF PARAPET WALL.
  - 4 ROOF CRICKET. 1/2" ROOF SHEATHING OVER METAL STUD DIAGONAL BRACING SLOPE AT ONE TO ONE.
  - 5 TAPERED RIGID INSULATION CRICKET WITH 1/4" ISOLATION BOARD (TYP).
  - 6 ROOF DRAIN AND OVERFLOW IN RECESS. (16"W X 8"H) THROUGH WALL OVERFLOW SCUPPER. SPILL TO ROOF BELOW.
  - 7 OUTLINE OF UPPER ROOF AT ENTRY TOWER. SEE UPPER ROOF PLAN.
  - 8 30" SQUARE ACCESS PANEL.
  - 9 24" SQUARE ACCESS PANEL.
  - 10 ROOF ACCESS HATCH AND LADDER.
  - 11 EXHAUST FAN UNIT.
  - 12 MECHANICAL UNIT TO BE CONCEALED FROM VIEW BY PARAPET. REFER TO MECHANICAL DRAWINGS FOR LOCATION & QUANTITY.
  - 13 3'-3" VENTS THROUGH ROOF FOR RECLAIM TANKS.
  - 14 1'-4" VENT STACK FOR RESTROOM.
  - 15 ROOF WALK PATH OVERLAY.
  - 16 ROOF VENT. FLASHVENT OR APPROVED EQUAL.
  - 17 OPENING IN WALL BETWEEN CRICKETS.
  - 18 WALL MOUNTED METAL CANOPY BELOW (TYP)
  - 19 CEMENT PLASTER FINISH AT TOP OF FURRED WALL FEATURE (TYP).
  - 20 WALL BELOW SHOWN DASHED
  - 21 UPPER PARAPET AND PILASTER DOWN LIGHTING.
  - 22 GUARDIAN FALL PROTECTION ANCHOR POST.

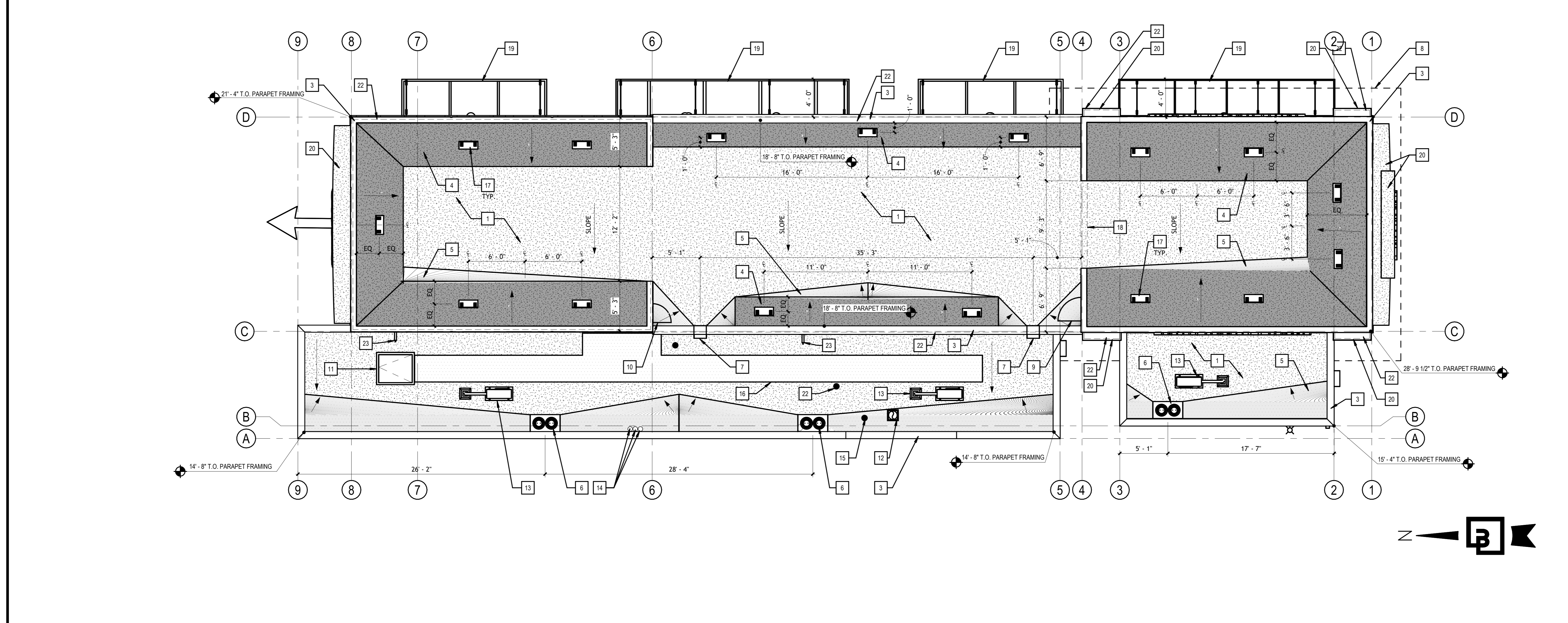
KEYNOTES

- GENERAL NOTES - ROOF
- A. ALL ROOF AREAS SHALL SLOPE TO FACILITATE WATER DRAINAGE. G.C. SHALL CONFIRM THAT ALL AREAS ARE PROVIDED WITH POSITIVE ROOF DRAINAGE PRIOR TO SHEATHING ROOF.
  - B. ROOF CONTRACTOR SHALL PROVIDE A CERTIFICATE STATING THAT THE ROOF SYSTEM HAS BEEN INSTALLED PER MANUFACTURER'S REQUIREMENTS.
  - C. G.C. SHALL CONFIRM THAT ALL ROOF DRAINS ARE LOCATED AT THE LOWEST POINT OF THE ROOF.
  - D. G.C. SHALL VERIFY SIZE AND LOCATION OF ALL ROOF OPENINGS, VENTS, PIPES, ETC. WITH ALL TRADES & OWNERS REP.
  - E. SEE MECHANICAL, ELECTRICAL, AND PLUMBING DRAWINGS FOR OPENINGS, DUCTS, AND VENTS THROUGH THE ROOF THAT MAY NOT BE SHOWN ON THE ARCHITECTURAL ROOF PLAN. PROVIDE FLASHING AND COUNTER-FLASHING PER ROOFING MANUFACTURER'S WRITTEN RECOMMENDATIONS.
  - F. FOR ROOF DRAINAGE CALCULATIONS SEE PLUMBING DWG'S. THIS INCLUDES ROOF DRAIN AND OVERFLOW SIZES ALONG WITH TRIBUTARY AREAS.
  - G. MEMBRANE ROOFING SHALL BE U.L. RATED CLASS 'A'.

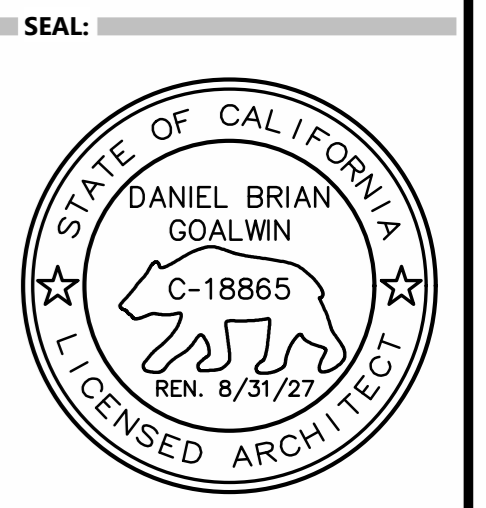
GENERAL NOTES - ROOF

SYMBOL	DESCRIPTION
	CANOPY ABOVE
	ROOF DRAIN AND OVERFLOW
	ROOF HATCH
	EXHAUST VENT
	VENT STACK
	ROOF VENT

ROOF SYMBOL LEGEND



ROOF PLAN 3/16" = 1'-0" 1



CONSULTANT:  
Consultant:

QUICK QUACK CAR WASH 26-643



3815 FLORIN ROAD  
SACRAMENTO, CA 95823

REV	DATE	DESCRIPTION

DRAWN BY: MB  
CHECKED BY: MJU  
ARCH. PROJECT NO.: 23559  
SHEET NAME: EXTERIOR ELEVATIONS  
SHEET NUMBER:

A3.10

SYMBOL	DESCRIPTION
	MATERIAL/COLOR TAG
	KEYNOTE TAG
	DETAIL INDICATOR
	DOOR SYMBOL - REFER TO DOOR SCHEDULE A7.10
	BUILDING SECTION

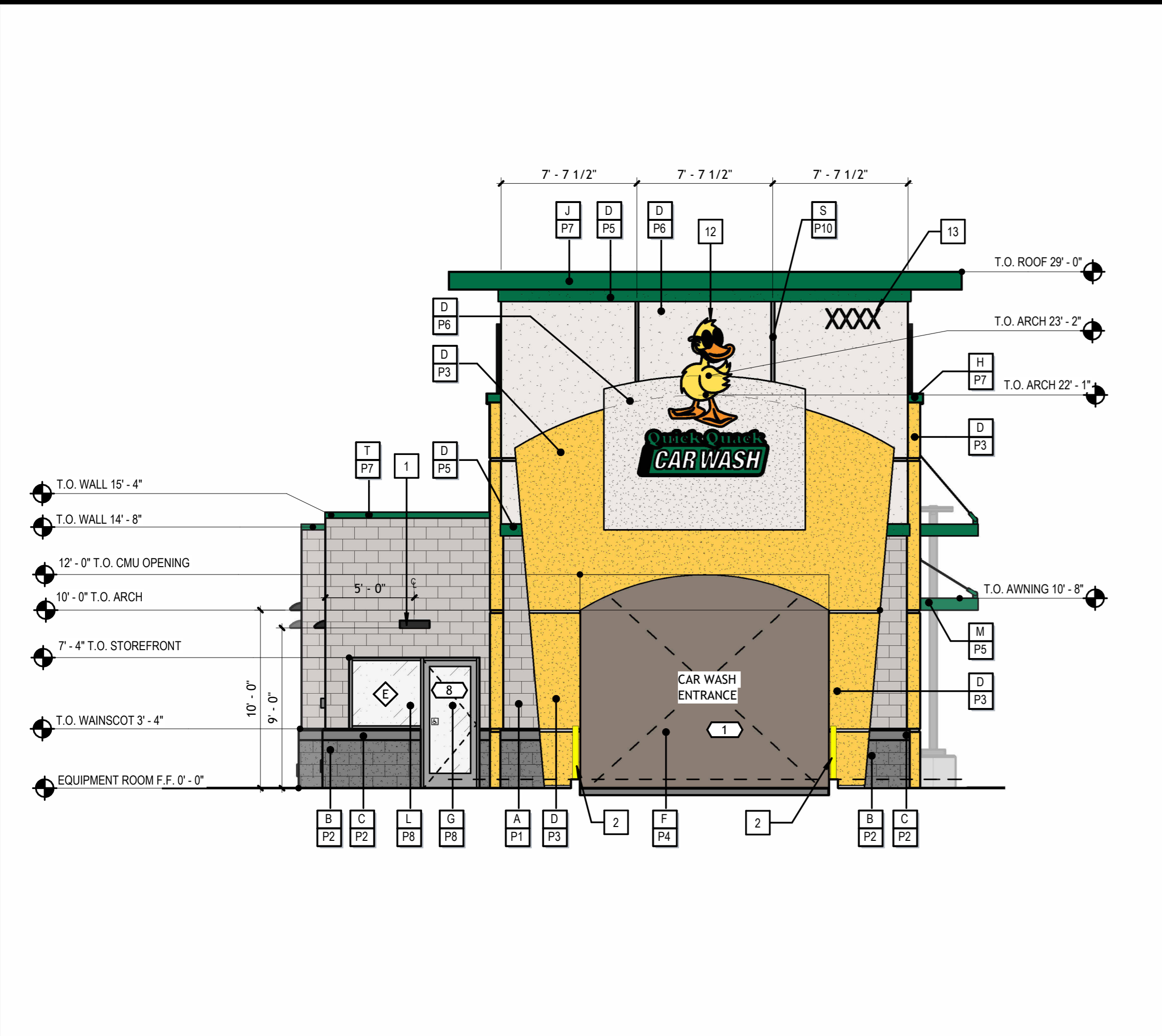
- KEYNOTE DESCRIPTION**
- WALL PACK LIGHT FIXTURE (DOWN ONLY). REFER TO ELEC. DWGS
  - 4" PIPE BOLLARD FILLED WITH CONCRETE.
  - ELECTRICAL SWITCHGEAR.
  - (16"W X 8"H) THROUGH WALL OVERFLOW SCUPPER. SPILL TO ROOF BELOW.
  - ROOF DRAIN THROUGH WALL AND OVERFLOW SCUPPER - PAINT TO MATCH ADJACENT.
  - 32" WIDE X 16" HIGH VENT - PAINT TO MATCH ADJACENT FINISH; LOUVERS NOT REQUIRED WHEN EQUIPMENT ROOM IS AIR CONDITIONED.
  - KNOX BOX TO BE 3200 SERIES, RECESSED AND INSTALLED AT A MIN. HT. OF 5'-0" TO 6'-0" MAX. THE FIRE DEPT. ALERT DECAL IS TO BE MOUNTED ON THE DOOR OR FRAME OF BUILDING MAIN ENTRANCE. KEYS TO BE PLACED INTO THE KNOX BOX WILL BE DETERMINED BY JURISDICTION FIRE PREVENTION STAFF. ALL PER JURISDICTION FIRE DEPT. REQUIREMENTS.
  - LED LIGHT FIXTURE CONCEALED IN PARAPET TRIM. REFER TO ELEC. DWGS.
  - THROUGH WALL OVERFLOW DRAIN.
  - PAY CANOPY. SEE CANOPY DRAWINGS.
  - LED LIGHT FIXTURE AT PILASTER CONCEALED IN PARAPET TRIM. REFER TO ELEC. DWGS.
  - SIGNAGE BY OTHERS SHOWN DASHED - FOR REFERENCE ONLY UNDER SEPARATE PERMIT. GENERAL CONTRACTOR TO COORDINATE WITH SIGN COMPANY.
  - ADDRESS NUMBERS SHALL BE VISIBLE FROM THE STREET, SHALL CONTRAST WITH THEIR BACKGROUND AND SHALL BE INTERNALLY OR EXTERNALLY ILLUMINATED BY A DEDICATED LIGHT SOURCE.

**SYMBOL LEGEND**

MATERIAL	COLOR / FINISH
A SMOOTH HONED FACE CMU 8x8x16	P1 BASALITE INTEGRAL COLOR STANDARD #225 LIGHT GRAY, W/ INTEGRAL FACTORY "RAINBLOC" WATER REPELLENT ADMIXTURE. MORTAR TO MATCH W/ "RAINBLOC"
B SPLIT FACE CMU 8x8x16	P2 BASALITE INTEGRAL COLOR STANDARD #790 DARK GRAY, W/ INTEGRAL FACTORY "RAINBLOC" WATER REPELLENT ADMIXTURE. MORTAR TO MATCH W/ "RAINBLOC"
C HONED FACE CMU SILL	P3 SHERWIN WILLIAMS #6902 "DECISIVE YELLOW"
D ACRYLIC PLASTER COLOR COAT W/ SAND FINISH	P4 SHERWIN WILLIAMS #7025 "BACKDROP"
E HOLLOW METAL DOOR	P5 PANTONE 348 C CONCORD "QUACKAMOLE GREEN"
F OVERHEAD METAL DOOR	P6 SHERWIN WILLIAMS #7004 "SNOWBOUND WHITE"
G ALUMINUM STOREFRONT GLASS DOOR	P7 PREFINISHED METAL - TO MATCH "QUACKAMOLE GREEN"
H PILASTER CAP	P8 TEMPERED GLASS CLEAR ANODIZED FRAME
J METAL ROOF	P9 BLACK CERAMIC FRIT TEMPERED SPANDEL GLASS CLEAR ANODIZED FRAME
K ROLLED STANDING SEAM DECK	P10 CLEAR ANODIZED
L ALUMINUM STOREFRONT	
M WALL MOUNTED AWNING	
N METAL GATE	
P STEEL BEAM	
Q STEEL COLUMN	
R STEEL TUBE AND WIRE MESH	
S PLASTER REVEAL. 2" WIDE X 1/4" DEEP	
T PARAPET CAP.	

**KEYNOTES - ELEVATIONS**

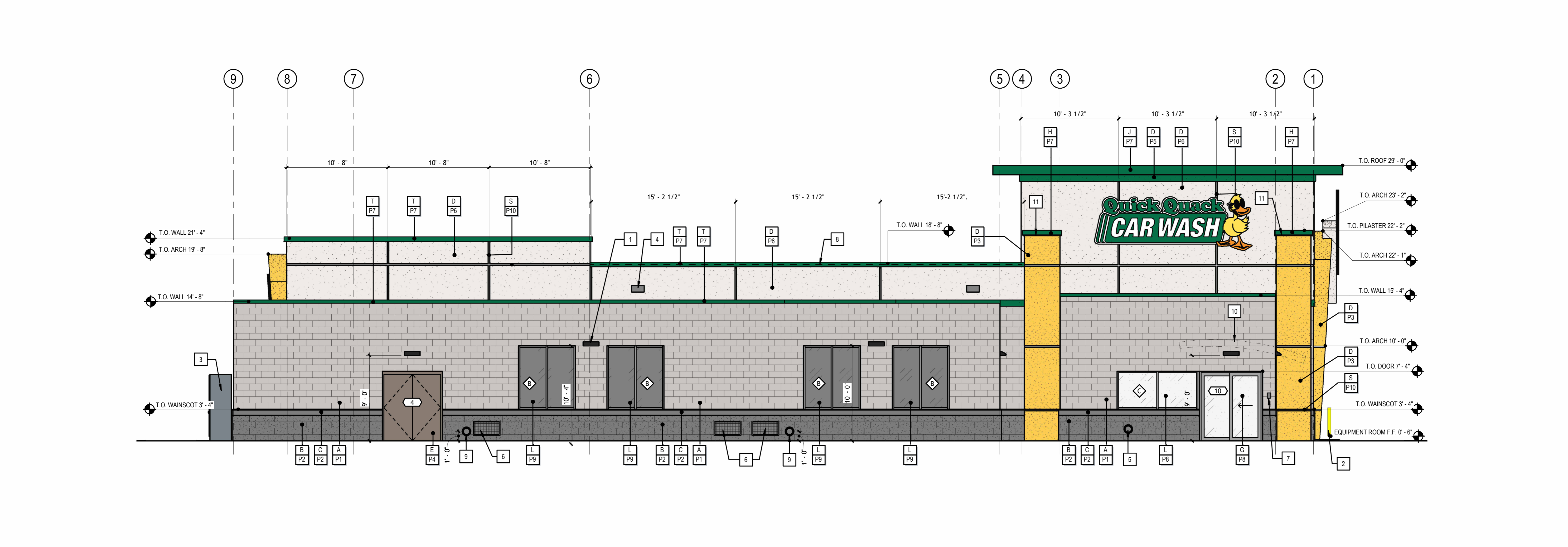
- GENERAL NOTES**
- REFER TO WALL LEGEND FOR PARTITION TYPES.
  - THE APPROVED AND STAMPED CONSTRUCTION DOCUMENTS SHALL REMAIN ON SITE AT ALL TIMES.
  - NOT USED -
  - IT IS THE INTENT THAT THIS PROJECT IS IN COMPLIANCE WITH ALL FEDERAL, STATE, AND LOCAL CODES AND REGULATIONS. THE GENERAL CONTRACTOR SHALL NOTIFY THE ARCHITECT IMMEDIATELY UPON THE DISCOVERY OF ANY DISCREPANCIES BETWEEN THE CONSTRUCTION DOCUMENTS. ANY WORK IN QUESTION SHALL NOT COMMENCE UNTIL WRITTEN CLARIFICATION IS ISSUED BY THE ARCHITECT.
  - REFER TO A2.14 FOR EQUIPMENT PLAN.
  - UNLESS NOTED OTHERWISE, MASONRY CONTROL JOINTS SHALL BE LOCATED SUCH THAT NO STRAIGHT RUN OF MASONRY WALL EXCEEDS 24'-0". REFER TO STRUCTURAL DWGS.
  - REFER TO A2.40 FOR INTERIOR FINISH SCHEDULE.
  - EXIT DOORS SHALL BE OPERABLE FROM THE INSIDE WITHOUT THE USE OF A KEY OR ANY SPECIAL KNOWLEDGE.
  - GC TO COORDINATE ALL REQUIRED BLOCKING FOR WALL HUNG EQUIPMENT, SHELVES, LIGHTING, ETC. FOR PROPER INSTALLATION HEIGHTS.
  - TACTILE EXIT SIGNS SHALL BE REQUIRED AT THE FOLLOWING LOCATIONS:
    - A. WHEREVER BASIC CBC PROVISIONS REQUIRE EXIT SIGNS FROM A ROOM OR AREA TO A CORRIDOR OR HALLWAY. THE TACTILE EXIT SIGN SHALL HAVE THE WORDS, "EXIT ROUTE."
    - B. EACH GRADE-LEVEL EXIT DOOR. THE TACTILE EXIT SIGN SHALL HAVE THE WORD, "EXIT."
  - OVERHEAD METAL DOORS TO BE SUPPLIED WITH A FACTORY FINISH FROM OVERHEAD DOOR MANUFACTURER IN EITHER STANDARD GRAY OR TAN TO MATCH ADJACENT WALL COLOR.
  - EXTERIOR SIGNAGE IS TO BE SUBMITTED, REVIEWED, AND APPROVED UNDER A SEPARATE PERMIT.



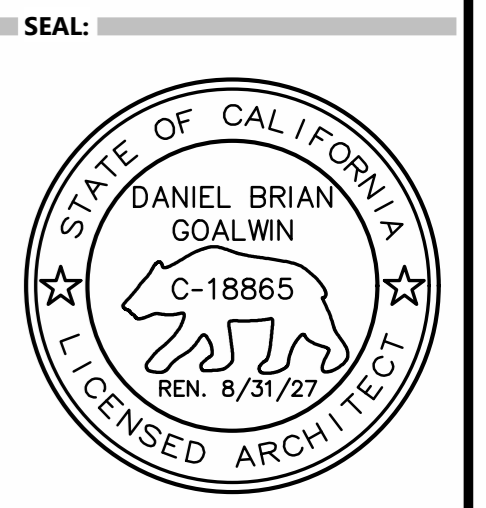
**SOUTH** 3/16" = 1'-0" 2

**EXTERIOR MATERIAL FINISHES & FINISH SCHEDULE**

**GENERAL NOTES - ELEVATIONS**



**WEST** 3/16" = 1'-0" 1



CONSULTANT:  
 Consultant:

**QUICK QUACK CAR WASH 26-643**



3815 FLORIN ROAD  
 SACRAMENTO, CA 95823

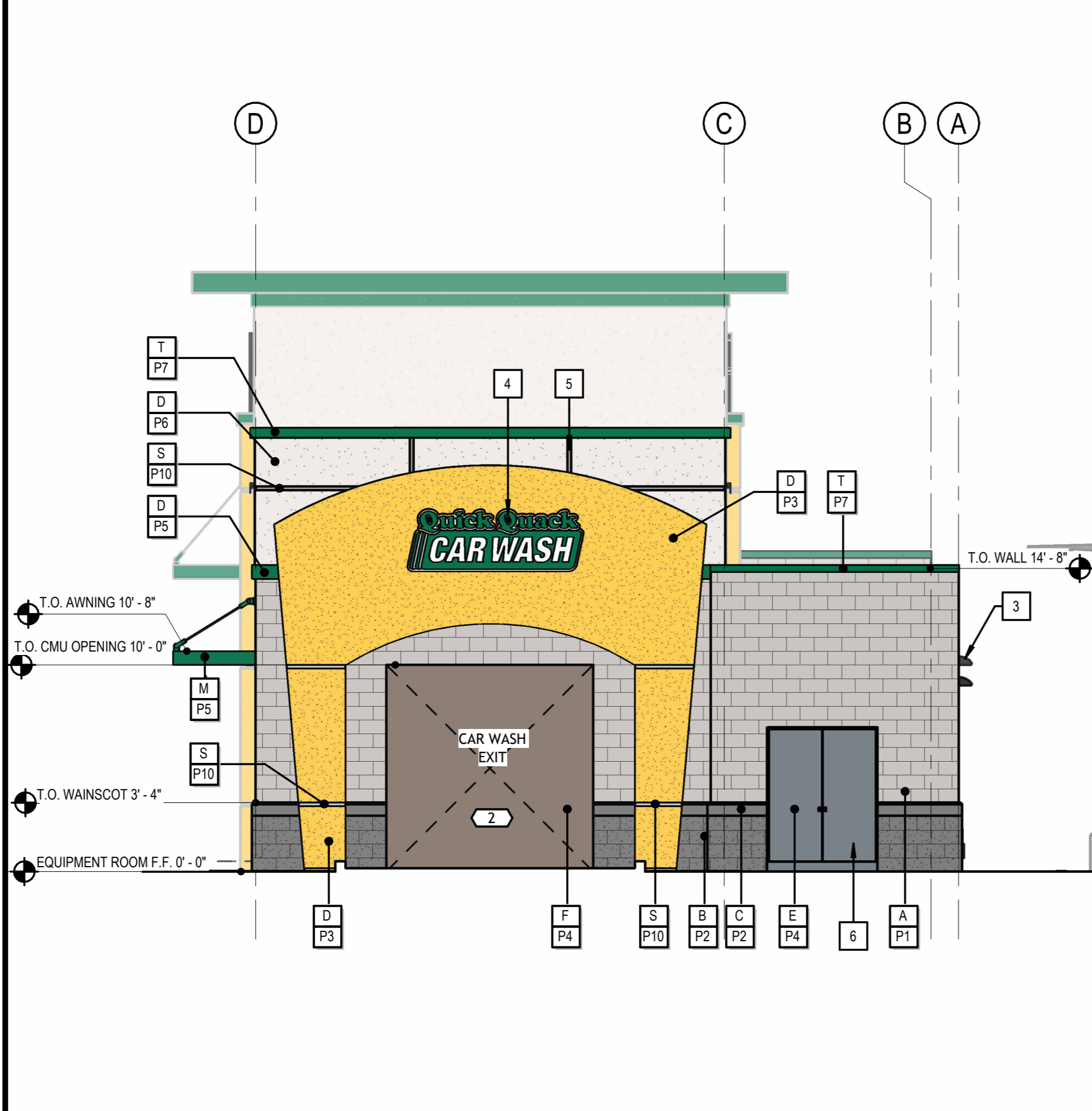
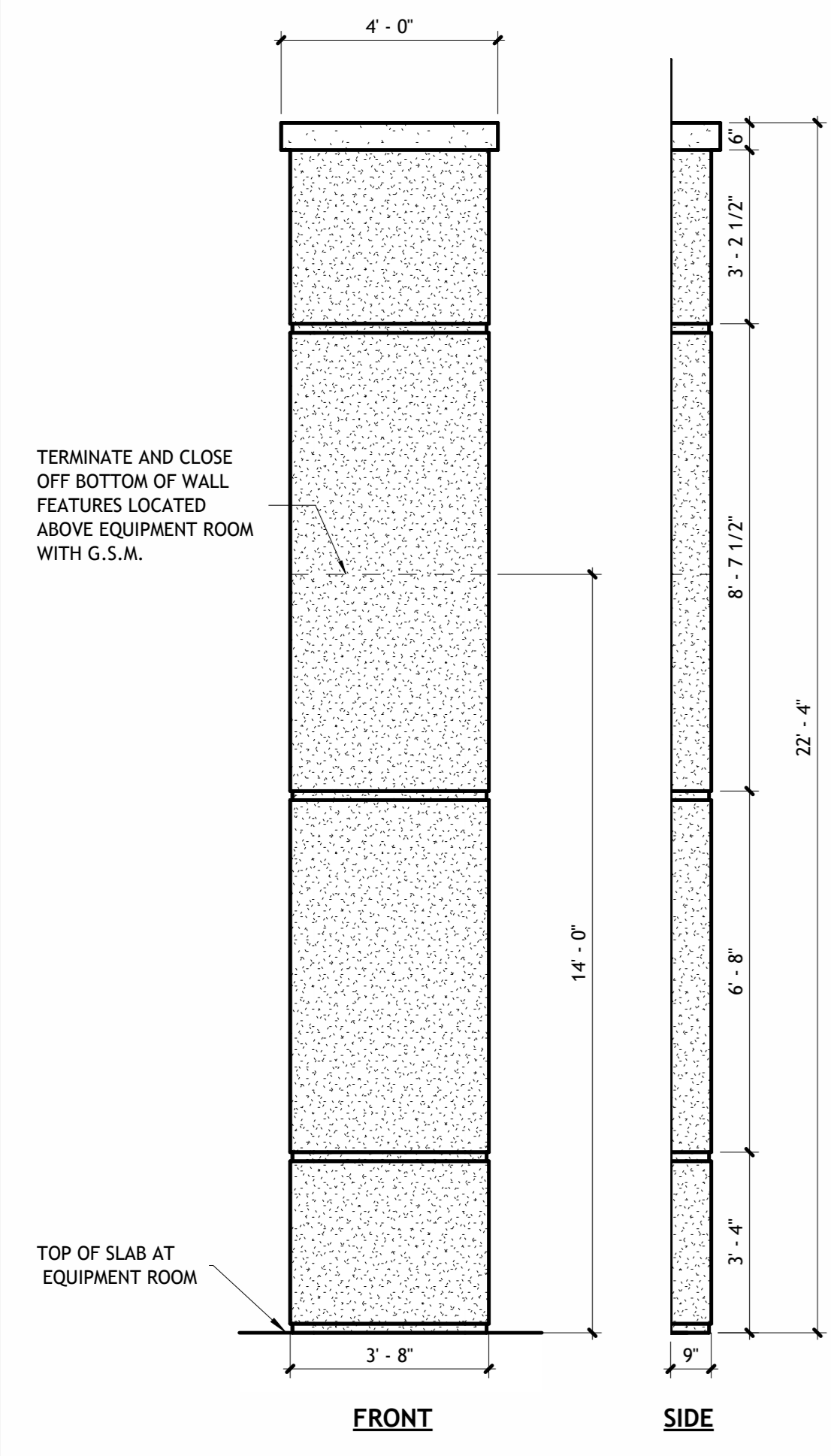
REV	DATE	DESCRIPTION

DRAWN BY: MB  
 CHECKED BY: MU  
 ARCH. PROJECT NO.: 23559  
 SHEET NAME: EXTERIOR ELEVATIONS  
 SHEET NUMBER: A3.11

- GENERAL NOTES**
- REFER TO WALL LEGEND FOR PARTITION TYPES.
  - THE APPROVED AND STAMPED CONSTRUCTION DOCUMENTS SHALL REMAIN ON SITE AT ALL TIMES.
  - NOT USED -
  - IT IS THE INTENT THAT THIS PROJECT IS IN COMPLIANCE WITH ALL FEDERAL, STATE, AND LOCAL CODES AND REGULATIONS. THE GENERAL CONTRACTOR SHALL NOTIFY THE ARCHITECT IMMEDIATELY UPON THE DISCOVERY OF ANY DISCREPANCIES BETWEEN THE CONSTRUCTION DOCUMENTS. ANY WORK IN QUESTION SHALL NOT COMMENCE UNTIL WRITTEN CLARIFICATION IS ISSUED BY THE ARCHITECT.
  - REFER TO A2.14 FOR EQUIPMENT PLAN.
  - UNLESS NOTED OTHERWISE, MASONRY CONTROL JOINTS SHALL BE LOCATED SUCH THAT NO STRAIGHT RUN OF MASONRY WALL EXCEEDS 24'-0". REFER TO STRUCTURAL DWGS.
  - REFER TO A2.40 FOR INTERIOR FINISH SCHEDULE.
  - EXIT DOORS SHALL BE OPERABLE FROM THE INSIDE WITHOUT THE USE OF A KEY OR ANY SPECIAL KNOWLEDGE.
  - GC TO COORDINATE ALL REQUIRED BLOCKING FOR WALL HUNG EQUIPMENT, SHELVES, LIGHTING, ETC. FOR PROPER INSTALLATION HEIGHTS.
  - TACTILE EXIT SIGNS SHALL BE REQUIRED AT THE FOLLOWING LOCATIONS:
    - A. WHEREVER BASIC CBC PROVISIONS REQUIRE EXIT SIGNS FROM A ROOM OR AREA TO A CORRIDOR OR HALLWAY. THE TACTILE EXIT SIGN SHALL HAVE THE WORDS, "EXIT ROUTE."
    - B. EACH GRADE-LEVEL EXIT DOOR. THE TACTILE EXIT SIGN SHALL HAVE THE WORD, "EXIT."
  - OVERHEAD METAL DOORS TO BE SUPPLIED WITH A FACTORY FINISH FROM OVERHEAD DOOR MANUFACTURER IN EITHER STANDARD GRAY OR TAN TO MATCH ADJACENT WALL COLOR.
  - EXTERIOR SIGNAGE IS TO BE SUBMITTED, REVIEWED, AND APPROVED UNDER A SEPARATE PERMIT.

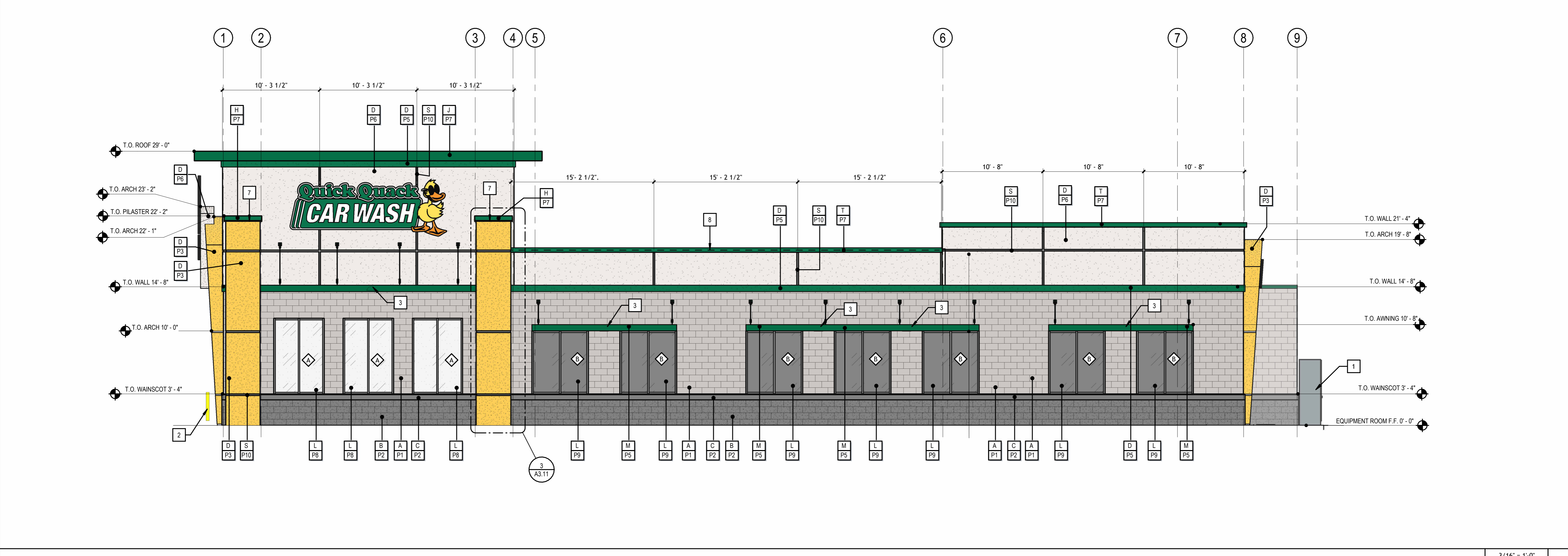
- KEYNOTES - ELEVATIONS**
- | # | KEYNOTE DESCRIPTION  |
|---|--|
| 1 | ELECTRICAL SERVICE SWITCHGEAR. SEE ELECTRICAL DRAWINGS.  |
| 2 | 4" PIPE BOLLARD FILLED WITH CONCRETE.  |
| 3 | WALL PACK LIGHT FIXTURE (DOWN ONLY). REFER TO ELEC. DWGS   |
| 4 | SIGNAGE BY OTHERS SHOWN DASHED - FOR REFERENCE ONLY UNDER SEPARATE PERMIT. GENERAL CONTRACTOR TO COORDINATE WITH SIGN COMPANY. |
| 5 | 2" WIDE X 1/4" DEEP PILASTER REVEAL.   |
| 6 | ELECTRICAL SWITCHGEAR.   |
| 7 | LED LIGHT FIXTURE AT PILASTER CONCEALED IN PARAPET TRIM. REFER TO ELEC. DWGS   |
| 8 | LED LIGHT FIXTURE CONCEALED IN PARAPET TRIM. REFER TO ELEC. DWGS.  |

MATERIAL	COLOR / FINISH
A SMOOTH HONED FACE CMU 8x8x16	P1 BASALITE INTEGRAL COLOR STANDARD #225 LIGHT GRAY, W/ INTEGRAL FACTORY "RAINBLOC" WATER REPELLENT ADMIXTURE. MORTAR TO MATCH W/ "RAINBLOC"
B SPLIT FACE CMU 8x8x16	P2 BASALITE INTEGRAL COLOR STANDARD #790 DARK GRAY, W/ INTEGRAL FACTORY "RAINBLOC" WATER REPELLENT ADMIXTURE. MORTAR TO MATCH W/ "RAINBLOC"
C HONED FACE CMU SILL	P3 SHERWIN WILLIAMS #6902 "DECISIVE YELLOW"
D ACRYLIC PLASTER COLOR COAT W/ SAND FINISH	P4 SHERWIN WILLIAMS #7025 "BACKDROP"
E HOLLOW METAL DOOR	P5 PANTONE 348 C CONCORD "QUACKAMOLE GREEN"
F OVERHEAD METAL DOOR	P6 SHERWIN WILLIAMS #7004 "SNOWBOUND WHITE"
G ALUMINUM STOREFRONT GLASS DOOR	P7 PREFINISHED METAL - TO MATCH "QUACKAMOLE GREEN"
H PILASTER CAP	P8 TEMPERED GLASS CLEAR ANODIZED FRAME
J METAL ROOF	P9 BLACK CERAMIC FRIT TEMPERED SPANDREL GLASS CLEAR ANODIZED FRAME
K ROLLED STANDING SEAM DECK	P10 CLEAR ANODIZED
L ALUMINUM STOREFRONT	
M WALL MOUNTED AWNING	
N METAL GATE	
P STEEL BEAM	
Q STEEL COLUMN	
R STEEL TUBE AND WIRE MESH	
S PLASTER REVEAL. 2" WIDE X 1/4" DEEP	
T PARAPET CAP.	



PILASTER DETAIL 3/8" = 1'-0" 3

NORTH 3/16" = 1'-0" 2



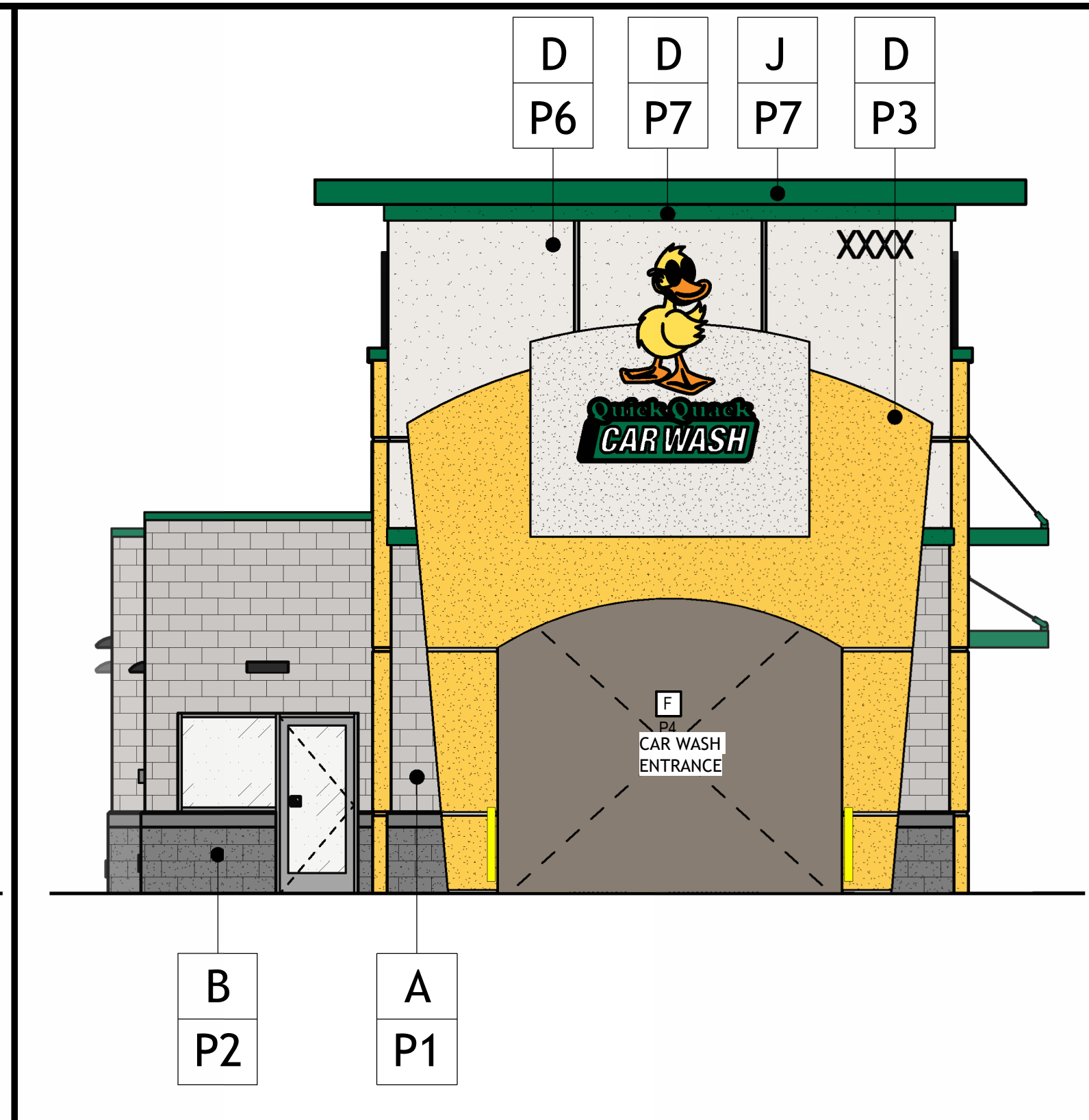
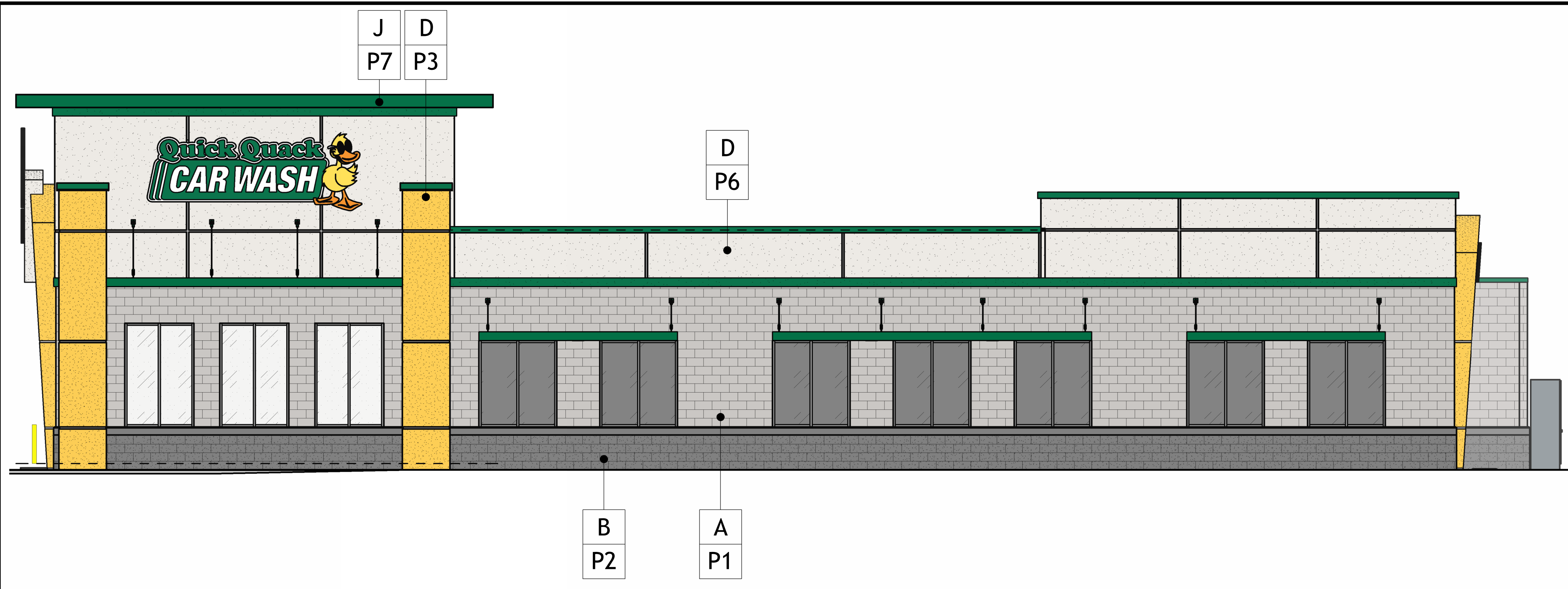
EAST 3/16" = 1'-0" 1

GENERAL NOTES - ELEVATIONS









EAST 3/16" = 1'-0" 2

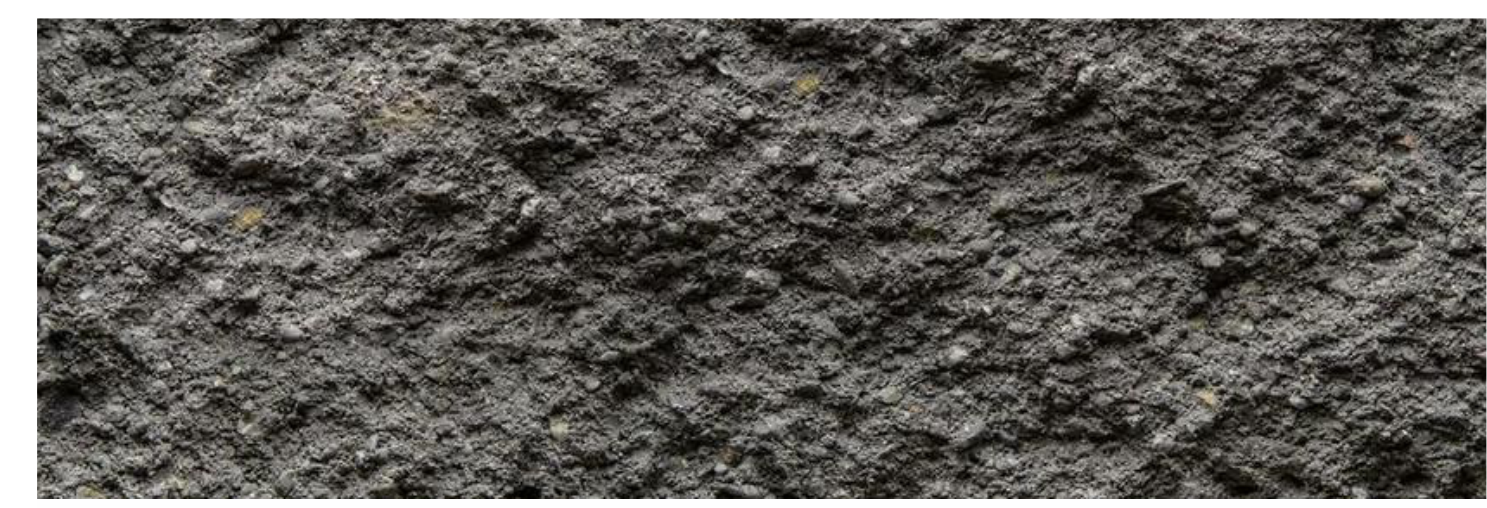
SOUTH 3/16" = 1'-0" 1

## EXTERIOR MATERIAL BOARD

- A SMOOTH HONED FACE CMU 8X8X16
- P1 BASALITE INEGRAL COLOR STANDARD #225 LIGHT GRAY



- B SPLIT FACE CMU 8X8X16
- P2 BASALITE INEGRAL COLOR STANDARD #790 DARK GRAY



- D ACRYLIC PLASTER COLOR COAT W/ SAND FINISH
- P3 SHERWIN WILLIAMS #6902 "DECISIVE YELLOW"



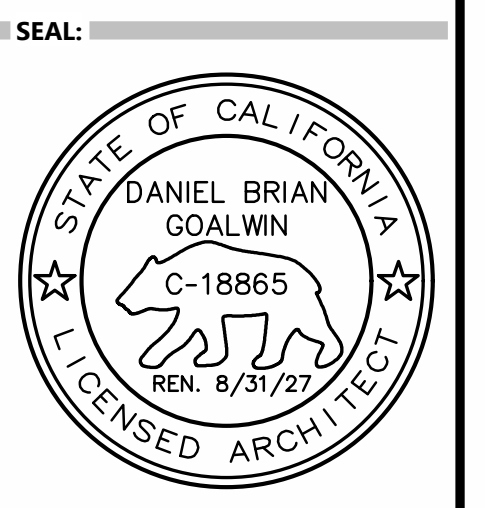
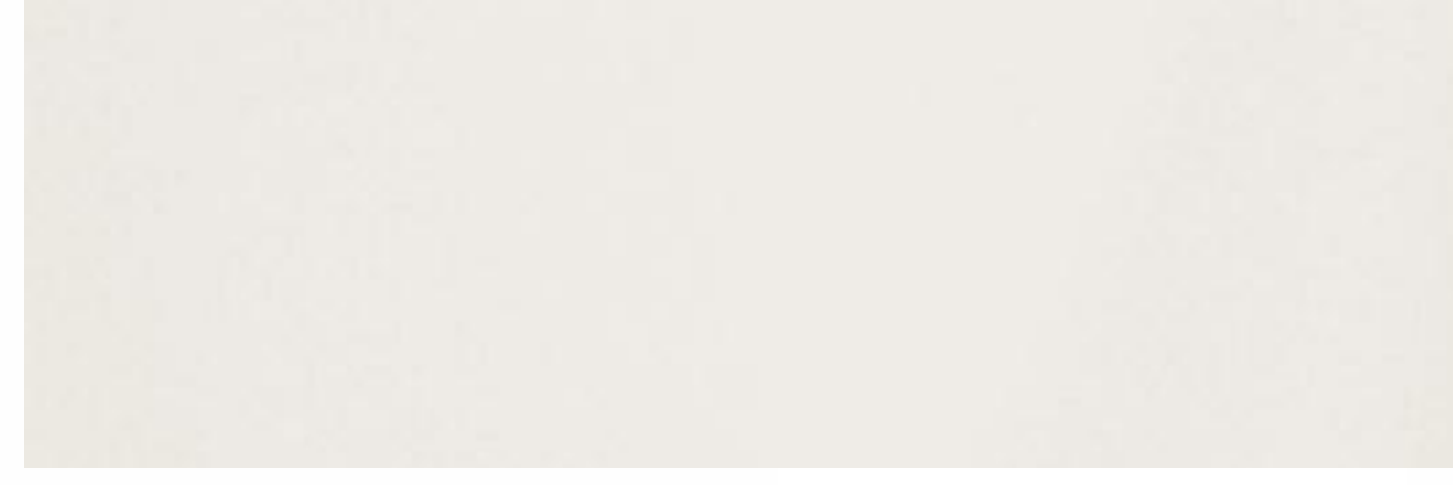
- D ACRYLIC PLASTER COLOR COAT W/ SAND FINISH
- P4 SHERWIN WILLIAMS #7025 "BACKDROP"



- D ACRYLIC PLASTER COLOR COAT W/ SAND FINISH
- P5 PANTONE 348 C CONCORD "QUACKAMOLE GREEN"
- J METAL ROOF
- P7 PREFINISHED METAL - TO MATCH "QUACKAMOLE GREEN"



- D ACRYLIC PLASTER COLOR COAT W/ SAND FINISH
- P6 SHERWIN WILLIAMS #7004 "SNOWBOUND WHITE"



CONSULTANT:  
Consultant:

**QUICK QUACK CAR WASH 26-643**



3815 FLORIN ROAD  
SACRAMENTO, CA 95823

REV	DATE	DESCRIPTION

DRAWN BY: JK/MB  
CHECKED BY: JK  
ARCH. PROJECT NO.: 23559  
SHEET NAME:

MATERIALS  
SHEET NUMBER: **A9.03**



February 3, 2026

VIA EMAIL

RE: Florin Road Quick Quack Carwash (P25-013)

Dear Danny Abbas,

Civic Thread (formerly Walk Sacramento) has reviewed the project routing for the proposed Florin Road Quick Quack Carwash (Project File No. P25-013), located near the intersection of Florin Road and Franklin Boulevard. Based on our review, Civic Thread does not recommend the proposal as it is inconsistent with adopted multimodal, safety, and climate goals for the Florin Road corridor.

This location is within a highly multimodal corridor that has been identified by the City as a priority area for safety improvements, mode shift, and investment in high-quality transit. Florin Road is part of the City's high-injury network under Vision Zero and is served by existing and planned high-frequency transit, including Bus Rapid Transit.

In this context, the proposed car wash use is fundamentally auto-oriented and does not meaningfully support walking, bicycling, or transit access. The project introduces additional on-site queuing, and driveway activity along a corridor where reducing conflicts and improving safety for people outside of vehicles is a stated City goal. The use does not provide pedestrian-oriented destinations or contribute to an active, comfortable streetscape that aligns with the City's long-term vision for Florin Road.

This proposal is similar to a car wash project previously reviewed for this location in 2019. At that time, City staff recommended denial based on conflicts with adopted plans and policies, and community organizations, including Walk Sacramento, submitted letters supporting that recommendation. The underlying conditions and policy framework for the corridor have not changed in ways that would alter those recommendations, and if anything, the City's commitments to Vision Zero, climate action, and transit-supportive land use have only strengthened since then.

More broadly, the Florin Road corridor and surrounding area plans envision land uses that can evolve over time to support a safe, walkable, and transit-oriented environment, particularly at prominent intersections. Approving an auto-service use at this location would limit future opportunities for development that better aligns with these adopted goals and planned public investments.

For these reasons, Civic Thread views the proposal as being inconsistent with adopted multimodal, safety, and climate goals for the Florin Road corridor. The organization also supports Planning staff's recommendation of denial and concurs that the proposed auto-oriented use is incompatible with the long-term vision for this location.

We appreciate the opportunity to comment. Please notify Civic Thread of future routings or notices for this project.



Sincerely,

Niru Vidyarthi  
Project Manager

**From:** [LU Project Review Account](#)  
**To:** [Daniel Abbes](#)  
**Subject:** RE: Project Routing: P25-013, Florin Road Quick Quack Carwash  
**Date:** Thursday, August 28, 2025 11:15:33 AM  
**Attachments:** [image003.png](#)  
[image004.png](#)  
[image005.png](#)  
[image006.png](#)  
[image008.png](#)

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Good Morning Danny,

Thank you for providing Sac Metro Air District with this project routing for review. As a heads up, I am leaving town for a couple of weeks soon, with some limited availability, but due to workload issues here at Sac Metro Air District, nobody will be available to cover my projects while I am gone. Therefore, I will be handling all projects I receive during that time as necessary with a more streamlined review.

As such, we maintain our project comments from the March 2025 pre-application meeting for this project, including comments about the project location in Sacramento's [AB 617 Community Air Protection Program](#) community, and along the Florin Corridor that is included in SacRT plans for Bus Rapid Transit (BRT). Ideally the site location on a prominent corner in this area would be a pedestrian-oriented use rather than an automobile-oriented use. This would be consistent with the public health aims of AB 617 and would support sustainable transportation investments such as BRT. We urge the City to continue to work on the potential of incorporating more pedestrian-oriented uses into the project site.

As a reminder, all projects are subject to Sac Metro Air District rules and regulations in effect at the time of construction. Please visit our website to [find a list of the most common rules that apply at the construction phase of projects](#). Projects undergoing any environmental review must implement Sac Metro Air District [Basic Construction Emission Control Practices](#), also available on our website, to help avoid significant construction-related air quality impacts. Please let me know if you have any questions.

All the Best,

**Molly Wright**

Air Quality Planner/Analyst

Desk: (279) 207-1157

[mwright@airquality.org](mailto:mwright@airquality.org)

[www.AirQuality.org](http://www.AirQuality.org)

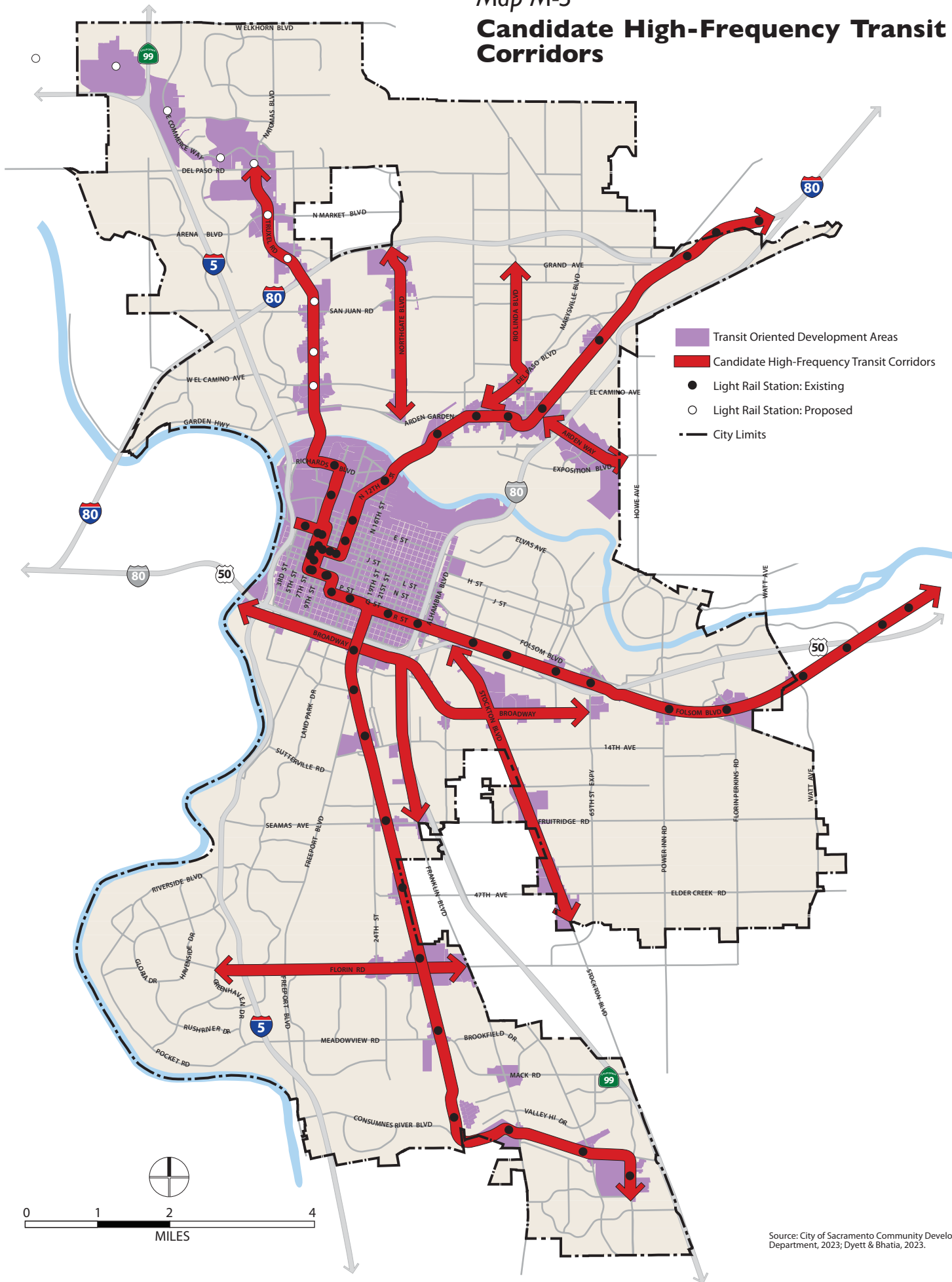


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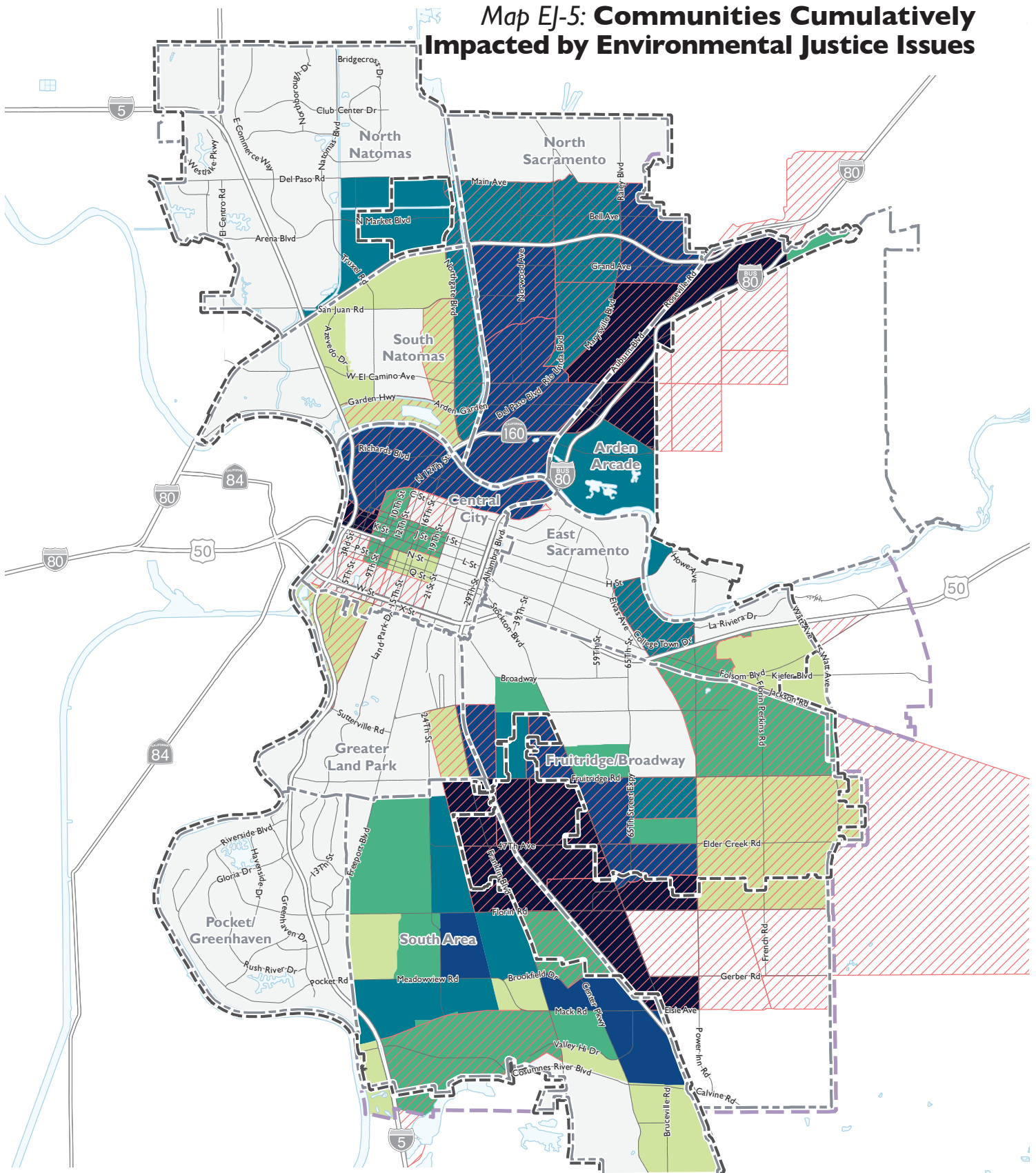
**From:** Daniel Abbes <DAbbes@cityofsacramento.org>

# Map M-3

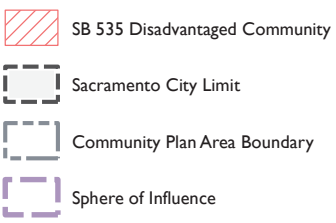
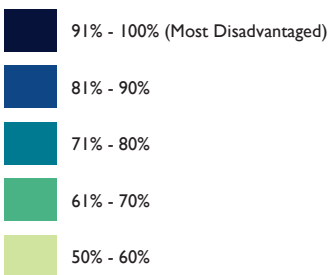
## Candidate High-Frequency Transit Corridors



# Map EJ-5: Communities Cumulatively Impacted by Environmental Justice Issues

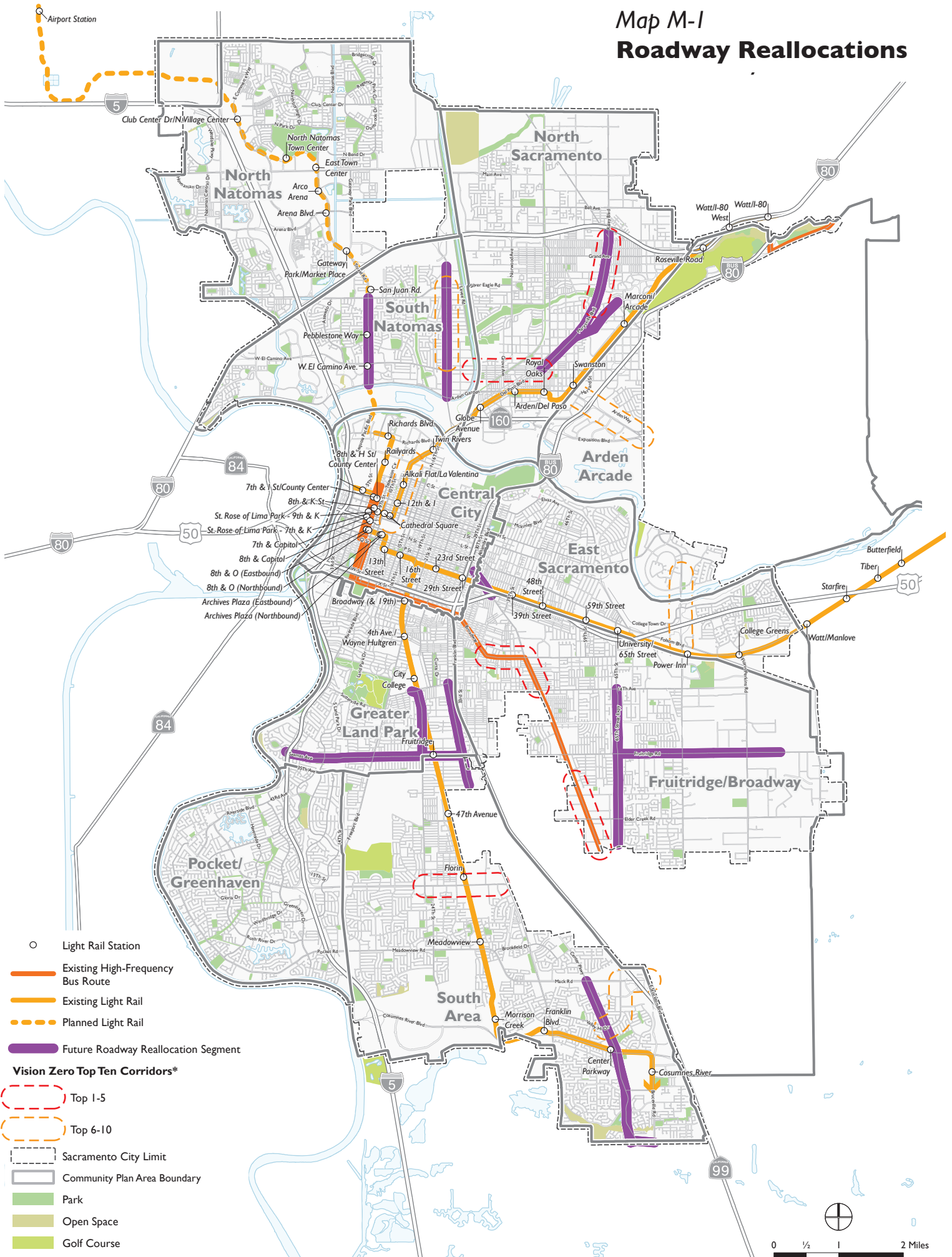


### EJ Composite Score (Top 50%)



Source: Eviction Lab, 2016; CalEPA, 2021; US Census Bureau ACS, 2016-2020; Datadiversitykids.org (Brandeis University), 2020; US EPA EJSCREEN, 2020; FEMA, 2020; CoStar, 2020; Urban Displacement Project (UC Berkeley), 2020; TIMS SafeTrac (UC Berkeley), 2020; Statewide Database (UC Berkeley), 2020; Trust for Public Land, 2021; NASA DEVELOP - Langley, 2021; HUD, 2022; US DHHS, 2021; City of Sacramento, 2023; Dyett & Bhatia, 2023

# Map M-1 Roadway Reallocations



- Light Rail Station
- Existing High-Frequency Bus Route
- Existing Light Rail
- - - Planned Light Rail
- Future Roadway Reallocation Segment

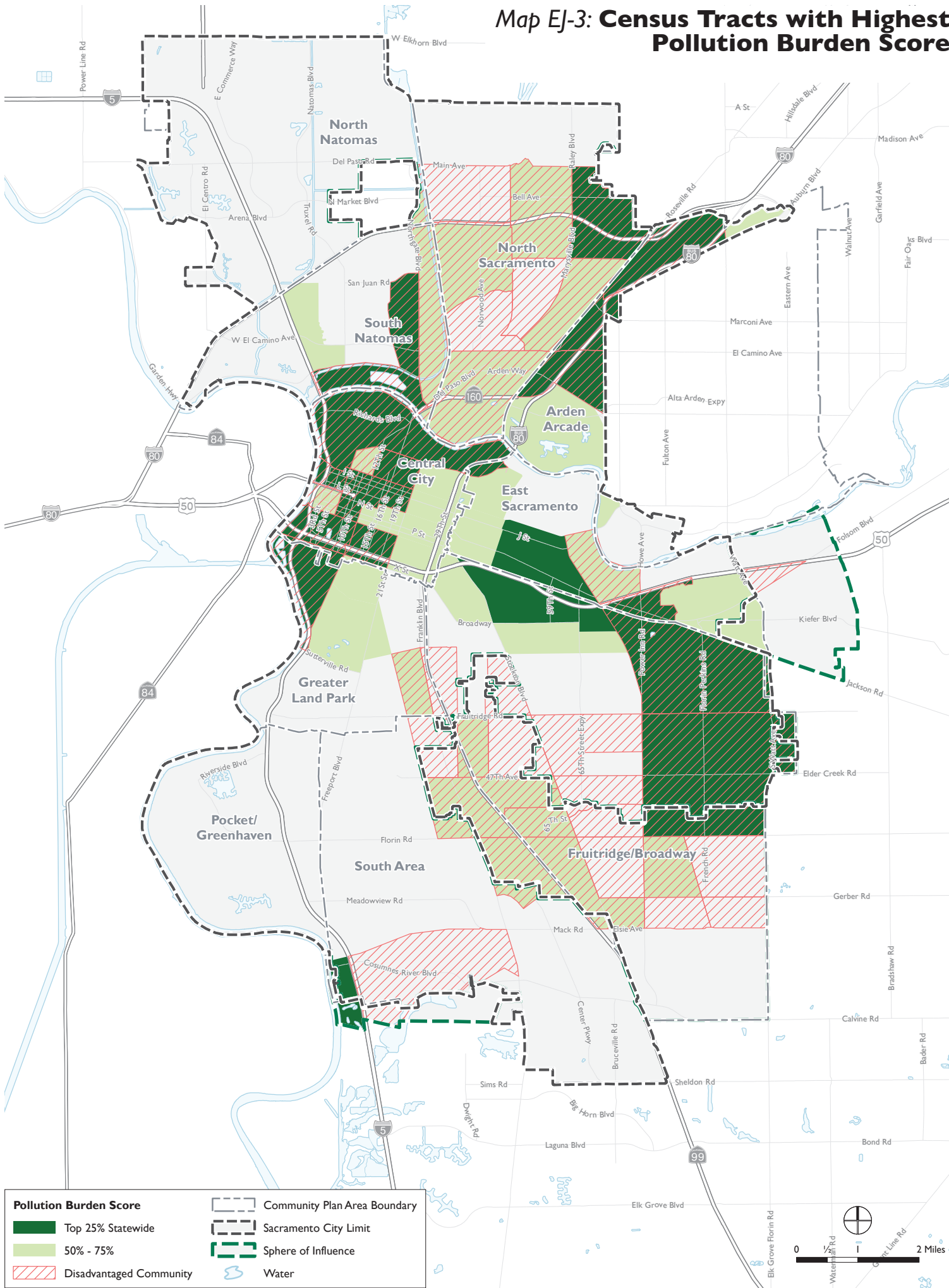
**Vision Zero Top Ten Corridors\***

- - - Top 1-5
- - - Top 6-10
- - - Sacramento City Limit
- - - Community Plan Area Boundary
- Park
- Open Space
- Golf Course

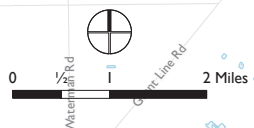


Source: Sacramento Regional Transit District, 2023; City of Sacramento, 2023; Fehr & Peers, 2023; Dyett & Bhatia, 2023

# Map EJ-3: Census Tracts with Highest Pollution Burden Score

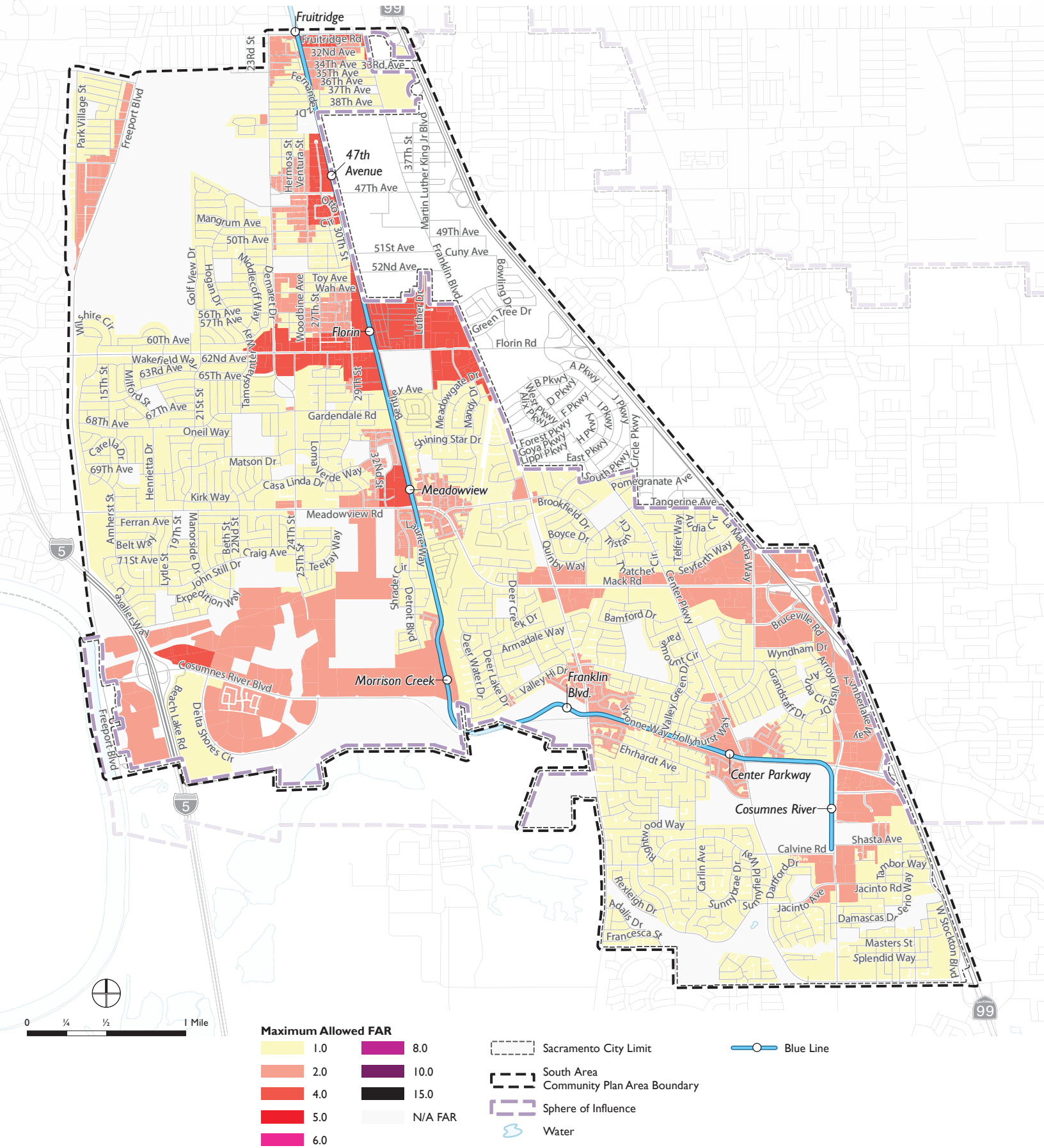


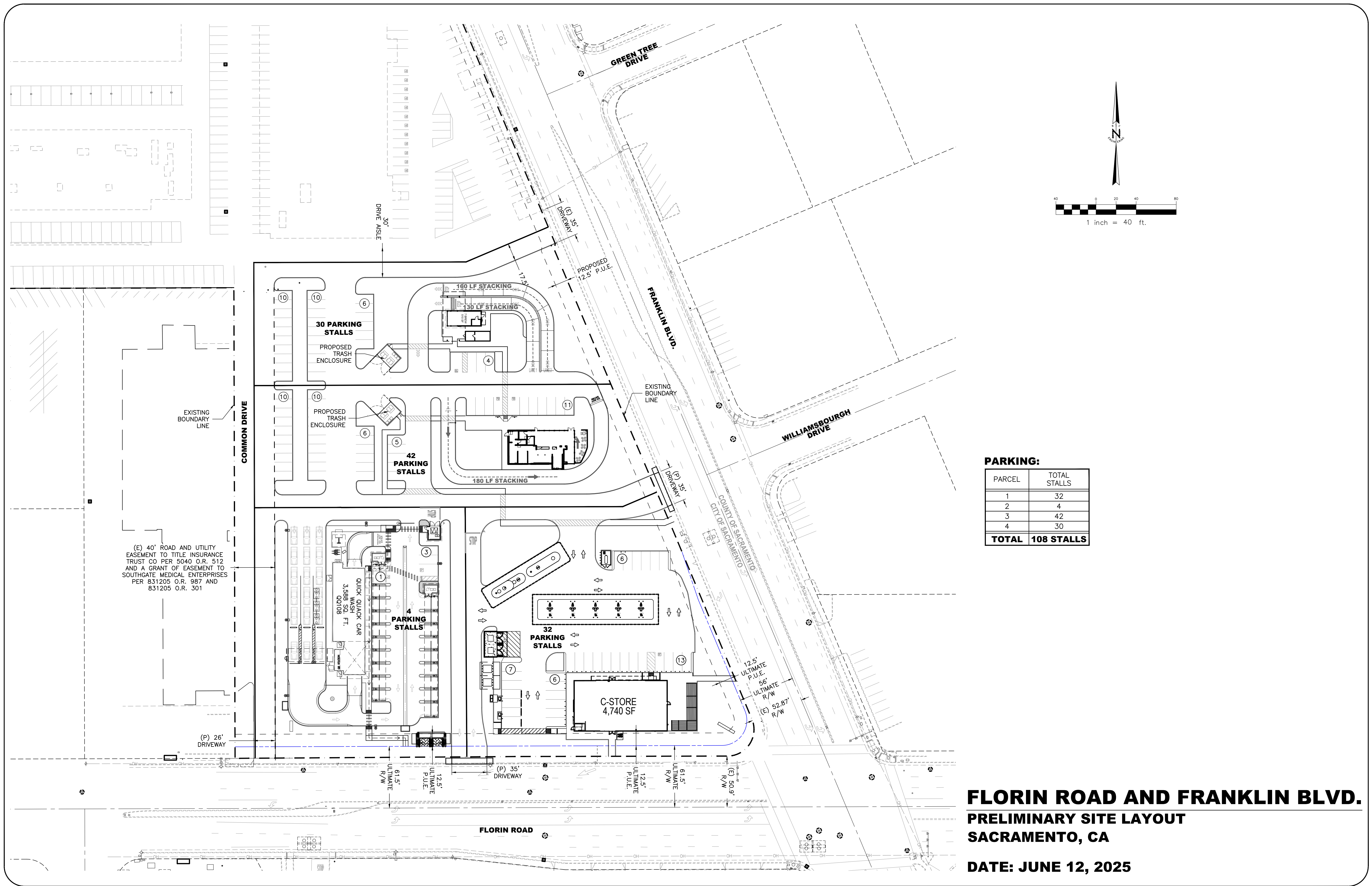
Pollution Burden Score		Community Plan Area Boundary	
<span style="display:inline-block; width:15px; height:15px; background-color:darkgreen;"></span>	Top 25% Statewide	<span style="border:1px dashed black; display:inline-block; width:15px; height:15px;"></span>	Community Plan Area Boundary
<span style="display:inline-block; width:15px; height:15px; background-color:lightgreen;"></span>	50% - 75%	<span style="border:1px dashed black; display:inline-block; width:15px; height:15px;"></span>	Sacramento City Limit
<span style="display:inline-block; width:15px; height:15px; border:1px dashed red;"></span>	Disadvantaged Community	<span style="border:1px dashed green; display:inline-block; width:15px; height:15px;"></span>	Sphere of Influence
<span style="display:inline-block; width:15px; height:15px; background-color:lightblue;"></span>	Water		



Source: CA OEHHA, 2021; CalEPA, 2022; City of Sacramento, 2023; Dyett & Bhatia, 2023.

# South Area Maximum Floor Area Ratio (FAR) - All Development





(E) 40' ROAD AND UTILITY EASEMENT TO TITLE INSURANCE TRUST CO PER 5040 O.R. 512 AND A GRANT OF EASEMENT TO SOUTHGATE MEDICAL ENTERPRISES PER 831205 O.R. 987 AND 831205 O.R. 301

**PARKING:**

PARCEL	TOTAL STALLS
1	32
2	4
3	42
4	30
<b>TOTAL</b>	<b>108 STALLS</b>

**FLORIN ROAD AND FRANKLIN BLVD.  
PRELIMINARY SITE LAYOUT  
SACRAMENTO, CA**

**DATE: JUNE 12, 2025**

## **Attachment 13**

Departments and agencies provide draft conditions to Planning staff for each project routed to them for review. The following are the draft conditions received by staff:

### **Public Works:**

1. Construct standard public improvements as noted in these conditions pursuant to Title 17 of the City Code. Improvements shall be designed to City Standards and assured as set forth in Section 17.502.130 of the City Code. All improvements shall be designed and constructed to the satisfaction of the Department of Public Works. Any public improvement not specifically noted in these conditions shall be designed and constructed to City Standards. This shall include the repair or replacement/reconstruction of any existing deteriorated curb, gutter and sidewalk adjacent to the subject property per City standards to the satisfaction of the Department of Public Works.
2. All right-of-way and street improvement transitions that result from changing the right-of-way of any street shall be located, designed and constructed to the satisfaction of the Department of Public Works. The center lines of such streets shall be aligned.
3. Dedicate sufficient right of way and construct Florin Road consistent with the adopted street section that accommodates a future Bus Rapid Transit lane. The adopted cross section is 123-foot right of way. This project will be held to 61.5-feet from the centerline to the back of walk along the project's frontage. The construction of the frontage improvements and associated travel lanes along Florin Road shall be to the satisfaction of the Department of Public Works.
4. All new driveways shall be designed and constructed to City Standards to the satisfaction of the Department of Public Works.
5. The applicant shall remove all unused existing driveways that are not aligned with the proposed site plan along the project frontage and reconstruct the curb, gutter, planter, and sidewalk to the satisfaction of the Department of Public Works.
6. Reciprocal access easements are required for shared use of the driveways w/ the adjacent parcel, if not already in place.
7. The minimum throat distance for the proposed project's driveway along Florin Road shall be at least 75-ft (throat distance is that distance a vehicle can move from the public right-of-way into a given site before encountering a conflict with parking stalls, aisles, etc).
8. The site plan shall conform to A.D.A. requirements in all respects. This shall include the replacement of any curb ramp that does not meet current A.D.A.

standards.

9. The site plan shall conform to the parking requirements set forth in City Code 17.608.040.
10. The applicant shall provide a signing and striping improvement plan if new signage or striping is proposed; or if existing signing and/or striping is removed or relocated. The plans shall be to the satisfaction of the Department of Public Works.
11. The site plan shall conform to the drive-thru requirements set forth in City Code 17.24.050.44.e.
  - a. A minimum stacking distance of one hundred eighty (180) feet shall be provided to each pick-up window or automated machine.
  - c. Entrances to drive-through lanes shall be at least twenty-five (25) feet from driveways entering a public or private street or alley.
  - d. The minimum width of each drive-through lane shall be eleven (11) feet. The entrance to the lane and the direction of traffic flow shall be clearly designated by signs and pavement marking or raised curbs.
12. The applicant shall monitor the operations of the drive-through to ensure that no vehicle will spill back onto the adjacent public right-of-way as a result of this project's on-site maneuvering to the satisfaction of the Department of Public Works.
13. Garbage pick-up and truck delivery services shall be prohibited within the hours of 7am to 9am and 4pm to 6pm.
14. The design of walls fences and signage near intersections and driveways shall allow stopping sight distance per Caltrans standards and comply with City Code Section 12.28.010 (25' sight triangle). Walls shall be set back 3' behind the sight line needed for stopping sight distance to allow sufficient room for pilasters. Landscaping in the area required for adequate stopping sight distance shall be limited 3.5' in height at maturity. The area of exclusion shall be determined by the Department of Public Works.

**Public Works Advisory:**

15. The proposed project is only being required to improve adjacent to the project's frontage with this phase of development. The City will require frontage improvements along the remainder of the parcel, access improvements, circulation requirements, and potentially additional traffic analysis once future development occurs for the overall parcel.

**SMUD:**

16. SMUD has existing underground 12kV facilities on northwest side of the PROJECT SITE that will need to remain. The Applicant shall be responsible for maintaining all CalOSHA and State of California Public Utilities Commission General Order No. 128 safety clearances during construction and upon building completion. If the required clearances cannot be maintained, the Applicant shall be responsible for the cost of relocation.
17. Structural setbacks less than 14-feet shall require the Applicant to conduct a pre-engineering meeting with all utilities to ensure property clearances are maintained.
18. Any necessary future SMUD facilities located on the Applicant's property shall require a dedicated SMUD easement. This will be determined prior to SMUD performing work on the Applicant's property. Applicant shall be responsible for confirming with SMUD appropriate/acceptable landscaping including placement within the easement area. SMUD reserves the right to prune & remove trees that encroach into the easement area. Applicant shall submit landscape improvement plans with tentative or final map as a condition of approval. New landscaping improvements shall be restricted to a maximum height of fifteen feet tall at full maturity.
19. In the event the Applicant requires the relocation or removal of existing SMUD facilities on or adjacent to the subject property, the Applicant shall coordinate with SMUD. The Applicant shall be responsible for the cost of relocation or removal. Applicant is further made aware that the proposed location of any relocated facilities will be subject to SMUD's vegetation management practices including restrictions of 15 feet high at full maturity and placement of trees within SMUD easements. Applicant shall bear cost to remove vegetation or trees located within proposed new facilities area and SMUD retains the right to engage in customary vegetation management practices at proposed new location after facility relocation. Applicant shall submit landscape improvement plans with tentative or final map as a condition of approval.
20. SMUD reserves the right to use any portion of its easements on or adjacent to the subject property that it reasonably needs and shall not be responsible for any damages to the developed property within said easement that unreasonably interferes with those needs, including but not limited to vegetation management, tree pruning or removal, weed abatement and application of weed abatement material, and a height restriction of fifteen feet tall at full maturity. Applicant shall submit landscape improvement plans with tentative or final map as a condition of approval.
21. The Applicant shall not place any building foundations within 5-feet of any SMUD trench to maintain adequate trench integrity. The Applicant shall verify specific

clearance requirements for other utilities (e.g., Gas, Telephone, etc.).

22. The Applicant shall comply with SMUD siting requirements (e.g., panel size/location, clearances from SMUD equipment, transformer location, service conductors). Information regarding SMUD siting requirements can be found at: <https://www.smud.org/en/Business-Solutions-and-Rebates/Design-and-Construction-Services>.
23. The Applicant shall dedicate a 12.5-foot public utility easement for overhead and/or underground facilities and appurtenances adjacent to all public street rights-of-ways. The 12.5-foot PUE shall be subject to SMUD's landscaping and tree placement guidelines within the easement area and such landscaping shall be subject to SMUD's vegetation management practices including but not limited to tree pruning, removal, and weed abatement and a height limit of fifteen feet at full maturity. Applicant shall submit landscape improvement plans with tentative or final map as a condition of approval.

#### **Fire Department:**

24. Timing and Installation. When fire protection, including fire apparatus access roads and water supplies for fire protection, is required to be installed, such protection shall be installed and made serviceable prior to and during the time of construction. California Fire Code Section 501.4
25. Upon submitting improvement plans for review, provide a water flow test. (Make arrangements with the Department of Utilities at 916-808-7890 or by email at [DOUdevelopmentreview@cityofsacramento.org](mailto:DOUdevelopmentreview@cityofsacramento.org), California Fire Code Section 507.4
26. Upon submitting improvement plans for review, identify that fire hydrants have been provided in accordance with California Fire Code Section 507 and Appendix C, Section C102.1 as amended the Sacramento City Code Section 507.5.1.
27. Upon submitting improvement plans for review, identify that appropriate Knox access for the site and any structures has been provided. California Fire Code Section 506
28. Upon submitting improvement plans for review, identify that an automatic fire sprinkler system shall be installed in any portion of a building when the floor area of the building exceeds 3,599 square feet or a change of use or occupancy that creates a greater fire or life safety hazard, and the floor area of that portion of the building, which is changed exceeds 3,599 square feet in area. California Fire Code as amended by the Sacramento City Code Amendments 903.2 (2&4) As measured on the Site Plan, the entire structure is approximately 3,605 square feet in size and requires automatic fire sprinkler protection per local ordinance.

Item 4 under 903.2 specifies that separation walls shall not apply to eliminate the installation of a fire sprinkler system.

29. Upon submitting improvement plans for review, identify that Fire Department Connections (FDCs) have been provided on address side of building no farther than 50 feet and no closer than 15 feet from a fire hydrant and not more than 30 feet from a paved roadway.
30. Upon submitting improvement plans for review, identify that an approved fire control room shall be provided for all buildings protected by an automatic fire extinguishing system. The room shall contain all system control valves, fire alarm control panels and other fire equipment required by the Fire Code Official. Fire Control rooms shall be located within the building at a location approved by the Fire Code Official, and shall be provided with a means to access the room directly from the exterior. Durable signage shall be provided on the exterior side of the access door to identify the fire control room. Fire Control rooms shall not be less than 50 square feet for rooms without a fire pump and 200 square feet when a fire pump is present. California Fire Code as Amended by the Sacramento City Code Section 901.4.7
31. Upon submitting improvement plans for review, provide a Site Safety Plan in compliance with Section 3303 of the California Fire Code. The plan shall identify at minimum, the following safety precautions during demolition and construction:
  - a. Name and contact information of the Owner's authorized agent (Site Safety Director) responsible for the development, implementation and maintenance of an approved written site safety plan.
  - b. Procedures for reporting emergencies.
  - c. Fire Department Access Routes.
  - d. Location of fire protection equipment, including type and size of fire extinguishers.
  - e. Smoking and cooking policies that include designated safe areas where smoking and cooking may occur with adequate signage in accordance with Section 3305.8
  - f. Location(s) and proper safety considerations for temporary heating and any associated equipment.
  - g. Hot Work Plan when any welding and/or cutting shall occur.
  - h. Means of providing safeguards to minimize the risk of unwanted releases, fires or explosions involving hazardous materials, such as ignitable

liquids/vapors or other combustible materials and ignition sources (cutting and welding, etc).

- i. Designated smoking areas free of ignitable vapors and other combustible materials.

### **Building Division Advisory:**

The plans are required to comply with the applicable sections of the California Building Standards Code (California Code of Regulations Title 24 Volumes 1-12).

### **Police Department:**

#### *Lighting*

32. Exterior lighting shall be white light using LED lamps with full cutoff fixtures to limit glare and light trespass. Color temperature shall be between 2700K and 4100K with a color rendering index of 80 or higher and a light loss factor of .95 or better. When choosing lamps, the applicant shall look for efficiency of 110 lumens per watt or better. All existing exterior fixtures shall be replaced with fixtures that meet this requirement.
33. Light poles, if applicable, shall be no higher than 20'.
34. Broken or damaged exterior lighting shall be repaired or replaced within 48 hours of being noted.
35. Entry drives, drive aisles, parking and bicycle parking shall be illuminated to a maintained minimum of 1.5-foot candles per square foot of parking area at a 6:1 average to minimum ratio.
36. Exterior walkways, alcoves and passageways shall be illuminated to a maintained minimum of 1/3 foot candles per square foot of surface area at a 6:1 average to minimum ratio.
37. Exterior lighting distribution and fixtures shall be approved by the Sacramento Police Department CPTED Sergeant (or designee) prior to issuance of a building permit.
38. Exterior lighting shall be designed in coordination with the landscaping plan to minimize interference between the light standards and required illumination and the landscape trees and required shading.
39. Exterior lighting shall be shielded or otherwise designed to avoid spill-over illumination to adjacent streets and properties.

## *Landscaping*

40. All mature landscaping shall follow the two-foot, six-foot rule. All landscaping shall be ground cover, two feet or less and lower tree canopies of mature trees shall be above six feet. This increases natural surveillance, eliminates hiding areas within the landscape, and provides for tenants and users a safer environment.
41. Tree canopies shall not interfere with or block lighting. This creates shadows and areas of concealment. The landscaping plan shall allow for proper illumination and visibility regarding lighting and surveillance cameras through the maturity of trees and shrubs.

## *Security*

42. Fencing, if applicable, shall be of decorative tubular steel, no climb type and a minimum of 6' in height.
43. Business shall be equipped with and maintain a security system with:
  - An alarm system with a valid UL Certificate in accordance with ANSI/UL Standard 681-2014 (Standard for Installation and Classification of Burglar and Holdup Alarm), Extent Number 4
  - a Video Assessment and Surveillance System (VASS)
44. Holdup alarm system shall be employed near the:
  - all points of sale
  - safe(s)
45. A Video Assessment and Surveillance System (VASS) shall be installed at the site and maintained by a property management company, security company, or designee.
46. Manager with access to VASS storage shall be able to respond to any activation within two hours.
47. Cameras shall be day/night capable with a resolution of no less than two (2) megapixels and a minimum frame rate of 15 frames per second.
48. Each driveway entrance and each building entrance shall be covered by a camera set at 100 pixels per foot or higher.
49. VASS shall be capable of exporting footage to common media in a standard viewing format and shall not require proprietary software for third party viewing.
50. VASS shall be placed in drive-thru areas so as to capture driver faces and vehicle license plates.

51. VASS shall be capable of storing no less than 30 days' worth of activity.

52. VASS shall provide comprehensive coverage of:

- all points of sale
- safe(s)
- manager's office
- parking lot
- adjacent public rights of way
- trash enclosure(s)
- areas not clearly visible from public streets
- coverage of all four (4) exterior sides of the building(s)

53. All dumpsters shall be kept locked or in locked enclosures. Gating for dumpster enclosures should be slatted to allow visual surveillance of the interior.

54. Any graffiti painted or marked upon the premises or on any adjacent area under the control of the applicant shall be removed or painted over with matching paint within 72 hours of being applied.

55. Exterior benches shall be constructed so as to deter skateboarding (e.g., center armrest partitions).

56. Property management shall be responsible for the daily removal of all litter from the site.

#### *During Construction*

57. The applicant shall enclose the entire perimeter of the project with a chain link fence with necessary construction gates to be locked after normal construction hours.

58. The location shall be monitored by security after normal construction hours during all phases of construction. This can be done via remote camera monitoring.

59. Adequate security lighting shall be provided to illuminate vulnerable equipment and materials. Lighting shall be white light with full cut off fixtures.

#### *Advisory Note*

60. City of Sacramento permits must be obtained for private patrol, alarms, and camera systems.

#### **Recycling and Solid Waste:**

61. Project must meet the requirements outlined in City Code Chapter 13.24 and 17.616.
62. The proposed trash enclosure must have sufficient space to store bins for trash, recycling, and organics. Containers may be used with an increase in collection frequency as an alternative to bins. Service level minimums for recycling and organics can be found in Chapter 13.24.600. Minimum service levels for recycling and organics can be found in Chapter 13.24.600. Trash and organics must be serviced at least weekly and recycling must be serviced at least monthly.
63. Solid waste trucks must be able to safely move about the project, with minimum backing, and able to empty the containers safely.
64. This project will be required to submit a Construction and Demolition (C&D) Debris plan, as outlined on the City's web site at <http://www.cityofsacramento.org/public-works/RSW/Collection-Services/Recycling/Construction-and-Demolition> as a condition of building permit issuance. Please contact the Solid Waste C&D team if you have any questions: Phone: (916) 808-0965; Email: [C&D@cityofsacramento.org](mailto:C&D@cityofsacramento.org)

**Youth, Parks, and Community Enrichment Advisory:**

65. As per City Code, the applicant will be responsible to meet their obligations regarding:

Title 18, 18.56 Park Development Impact Fee, due at the time of issuance of building permit. The Park Development Impact Fee is estimated at \$667. This project proposes a total of 3,337 square feet of Commercial Retail/Services development. The Park Development Impact Fee due for this project is based on the Housing Incentive Zone Rate of \$0.20 per square foot for retail and commercial services projects. Any change in these factors will change the amount of the PIF due. The fee is calculated using factors at the time that the project is submitted for building permit.



**Project Site – Present Day**









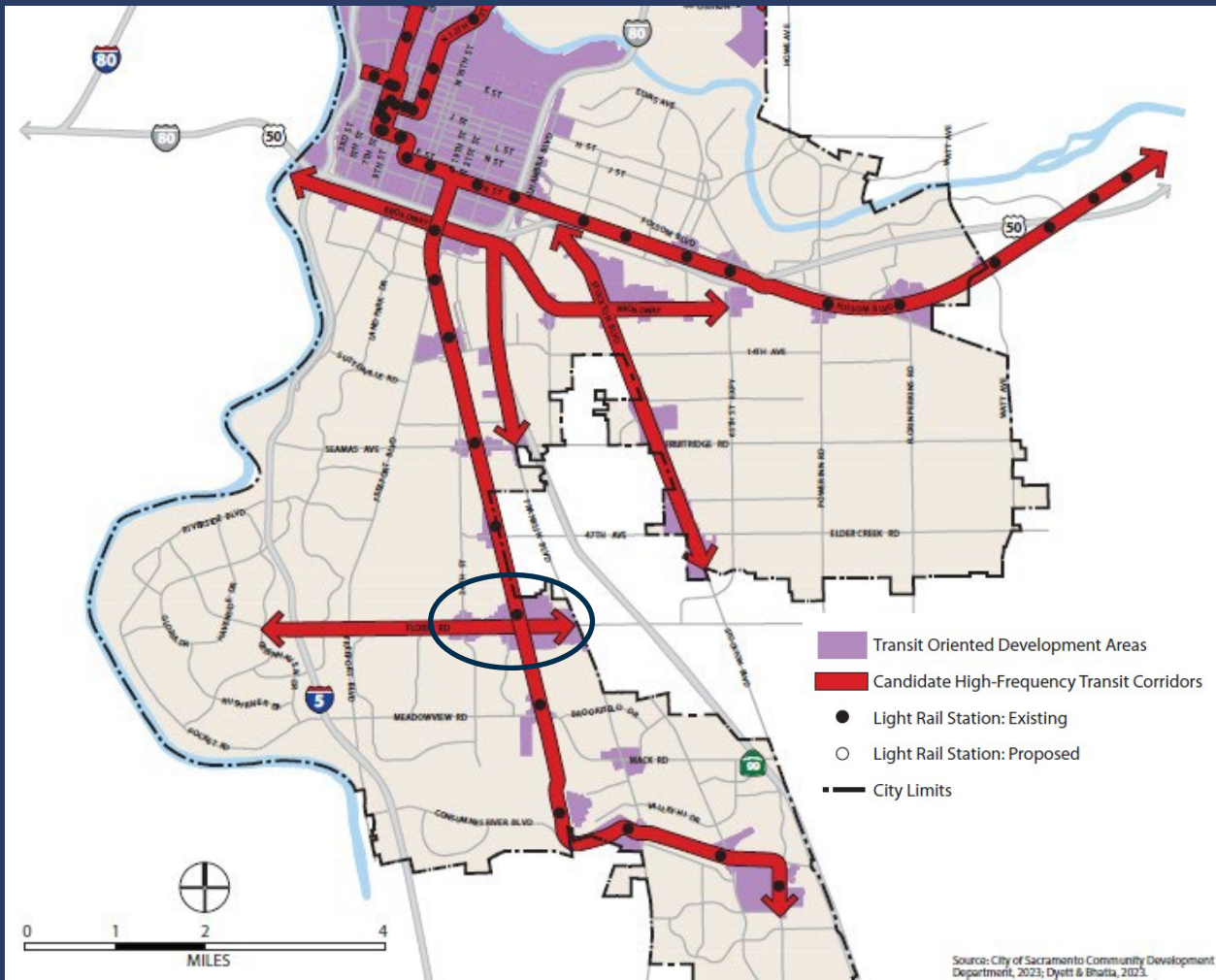


Presenter: Danny Abbess

A dark blue silhouette of the Sacramento skyline, including various buildings and the two towers of the Golden Gate Bridge, set against a light blue background.

Appeal: Florin Road Quick Quack Car Wash (P25-013)  
4-28-26 City Council meeting





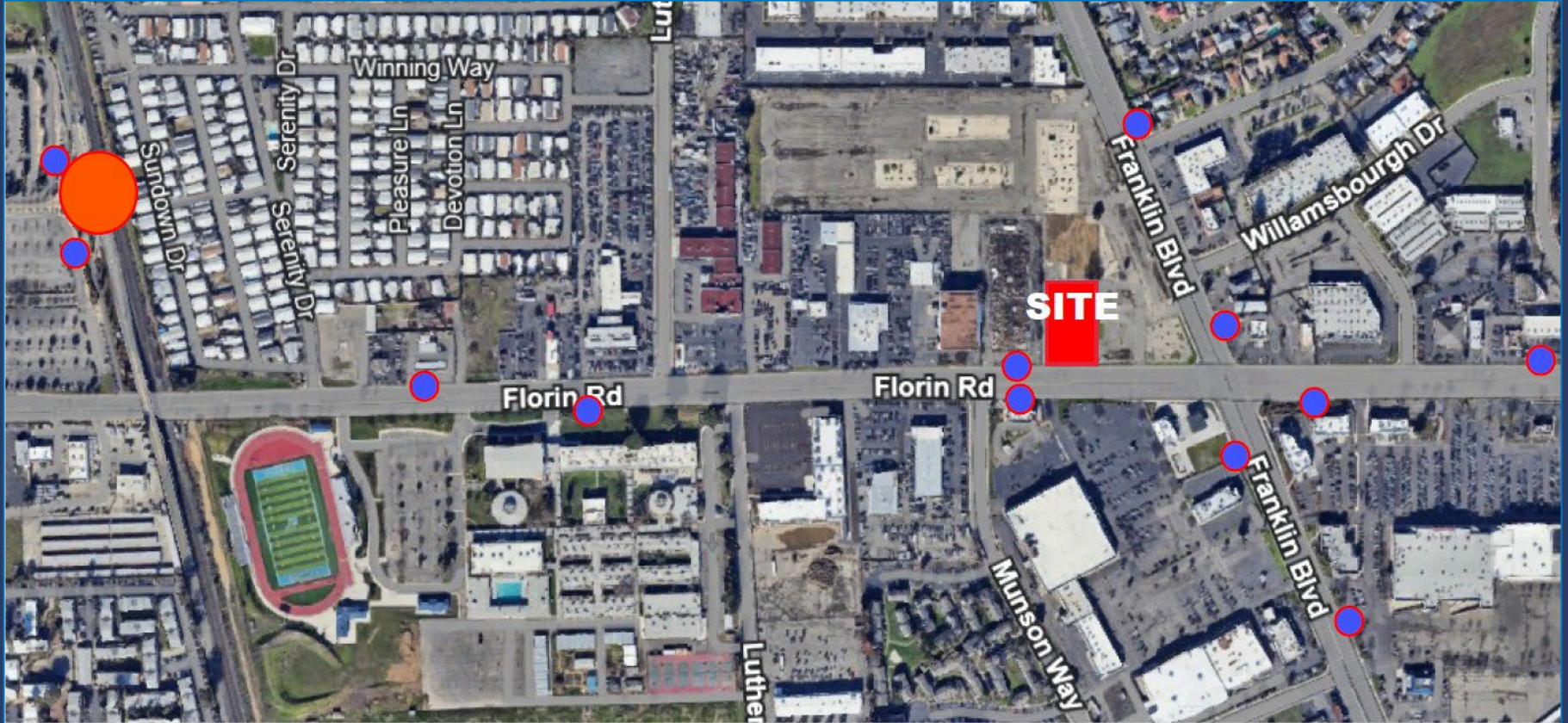
# Designated Transit- Oriented Development (General Plan Map M-3)

Source: City of Sacramento Community Development Department, 2023; Dyett & Bhatta, 2023.

# South Area Infill Vision

- “In the South Area, the SacRT Blue Line light rail and high frequency bus routes create opportunities for **transit-oriented infill** development of vacant and underutilized properties. Land use designations in these transit-oriented development areas are **intended to promote mixed-use** areas near light rail and bus service to **incentivize multi-unit housing** that supports more frequent/reliable transit service and bolsters retail vitality.” (Community Plan: South Area 11-SA-7)
- “Vacant and underutilized land has been redeveloped into vibrant mixed-use neighborhoods that feature a variety of **affordable and adaptable housing options** and **support high-quality transit** service along the SacRT Blue Line light rail and the Florin Road high-frequency bus corridor.” (Community Plan: South Area 11-SA-3)

# Transit Proximity and Access



# Florin Road Car Washes



(#1) Magic Car Wash & Detail – 2813 Florin Road – 0.8 miles away

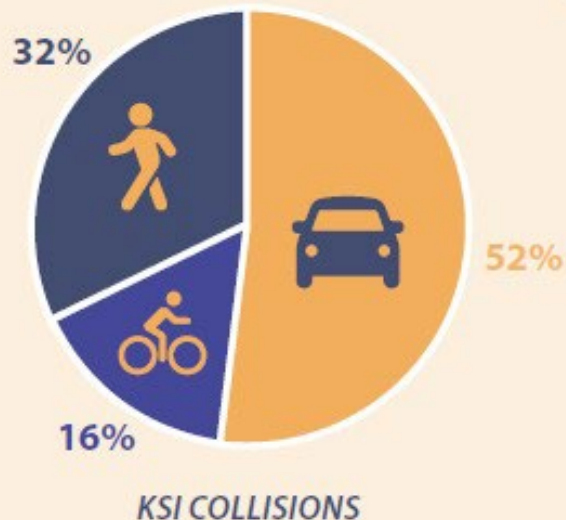
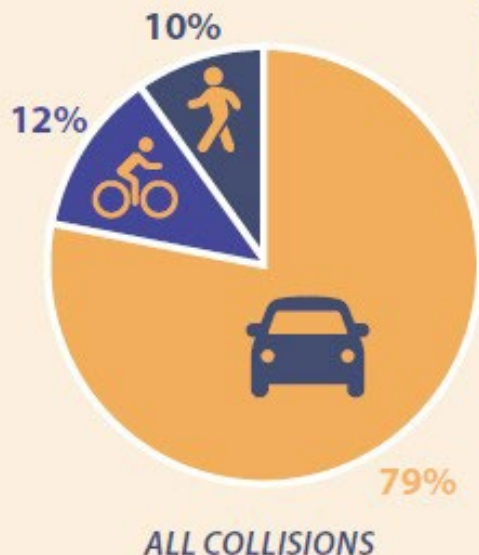
(#2) Magic Wand Car Wash – 3465 Florin Road – 0.35 miles away

(#3) Shell Car Wash – 4050 Florin Road – 0.03 miles away

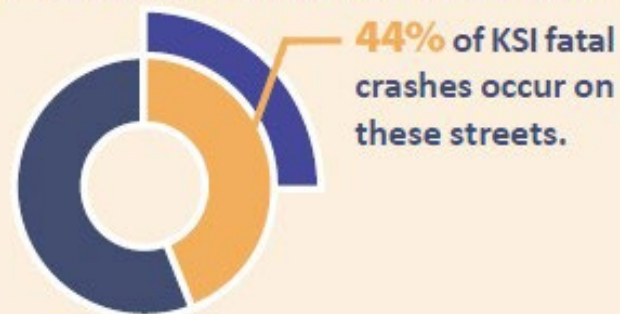
(#4) AMPM car wash – 4421 Florin Road – 0.32 miles away

## VISION ZERO TRAFFIC STATISTICS

Although cars are involved in the majority of traffic collisions in Sacramento, **half** of people killed or seriously injured (KSI) in crashes are bicyclists or pedestrians.

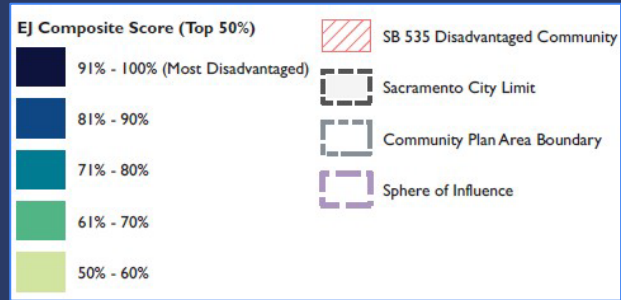
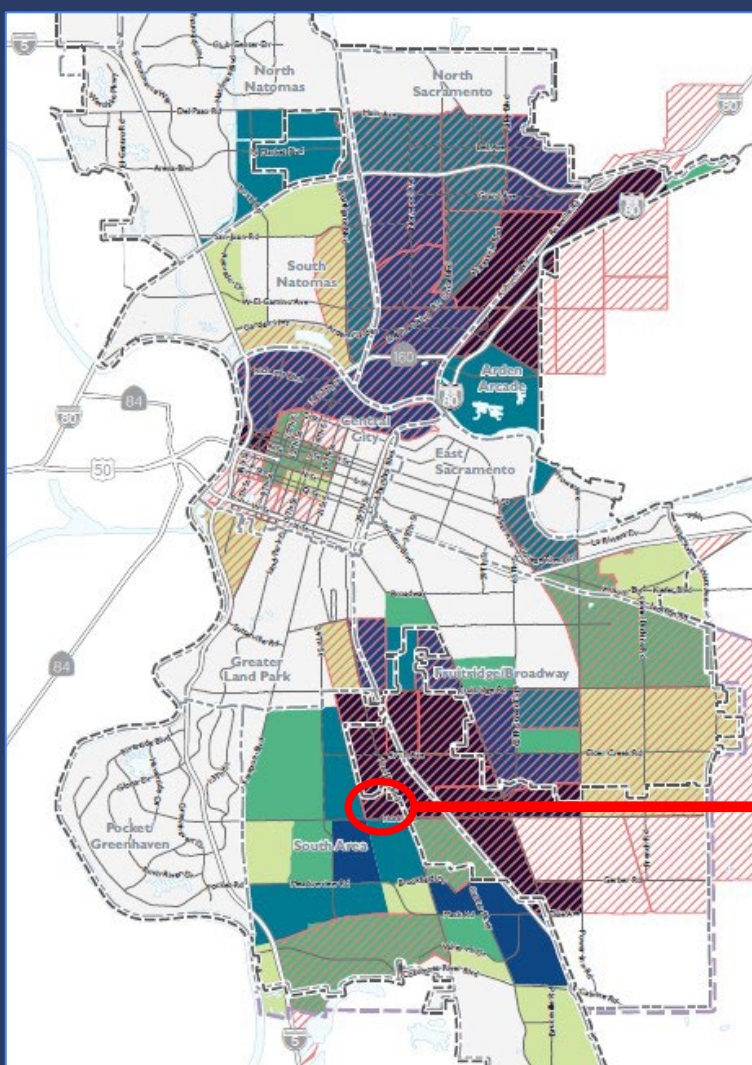


Although only **25%** of the City's roadway network are in a disadvantaged communities,



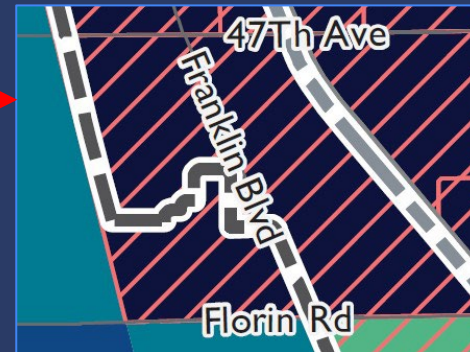
**54%** of pedestrian KSI crashes occurred within 200 feet of a transit stop.





# Most Disadvantaged Community

(General Plan Map EJ-5)



## Memorandum

S.O.I. Communications



To: Project Team

From: Kim Garrett, Project Manager

CC: File

### **Overview**

SOI Communications conducted community outreach activities for a proposed Quick Quack Car Wash located at the corner of Franklin Boulevard and Florin Road. The data collection was specific to the City of Sacramento residents in the Phoenix Park Neighborhood on Franklin and, the Rose Family Center located at 7700 Franklin Blvd. Inclusive in the data are three (3) self-identified residents who reside in the Morrison Creek Estates neighborhood immediately adjacent to Phoenix Park.

### **History and Methodology**

Starting January 30, 2025, and ending February 07, 2026, the outreach engaged respondents using a map and a contact script to convey a uniform message on the scope and location of the project. The script is also designed to limit predilections and inherent bias for or against the car wash and conditional use requests. Team members recorded feedback, comments, and questions. Respondents with strong opinions or who requested follow-up information received an additional interview from the project manager, Kim Garrett.

### **Summary**

Over the course of one year, the outreach team did not identify any pattern or trend of opposition. While the sampling did record requests for another use, respondents expressing their preference did not object to the use of the location as a Quick Quack car wash.

Older respondents produced the most robust responses. The older respondents, on average, recorded interviews that lasted 22 minutes. The longest interview was with an 80-year-old resident who chronicled the history of the property and the Florin/Franklin corridor. The interview lasted 47 minutes.

### **Responses to Quick Quack Car Wash**

The survey did not record a negative respond to the brand.

### **Results Summary**

- One hundred and seven contacts<sup>1</sup> made with fifty-six recorded responses.
- Most households recorded more than one response.
- Jobs: especially for students was the top response (location is close to Burbank High School)
- Respondents who wanted the use recorded having the service nearby was important
- On average, interviews last almost 8 minutes
- “What took so long,” was the top question
- The survey recorded only one “I don’t care” response.
- The sampling did not record any air quality concern(s)

Top Recorded Responses	
Response	No. of Responses
<i>Jobs that hire young men/women/students Hire locally</i>	18
<i>Need or want service</i>	15
<i>Clean up dirty lot/beautification</i>	7
<i>Healthy neighbor no liquor, no fast food</i>	6
<i>Support the community</i>	4
<i>Like brand</i>	3
<i>Get rid of homeless camping on vacant lot</i>	2
<i>Do not care</i>	1

### **Interview with Jackie Rose**

Jackie Rose founded the Rose Family Creative Empowerment Center in 2014 after retiring from the Sacramento Housing Redevelopment Agency. Her level of understanding of and working in the community (especially in Council Districts 5 and 8) is extensive. She is also uniquely positioned to understand the nexus between private investment and community enrichment. Ms. Rose expressed staunch support for the project.

Additionally, Ms. Rose requested direct outreach to Burbank High School students. The project applicant hosted a field trip to Quick Quack’s Center of Excellence which included presentations by an engineer, the project architect, regional operations manager, the availability of job training and the company’s need for the building and construction trades.

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<sup>1</sup> A household may have more than one contact attempt and, the sampling could record more than one respondent in a household. Teenagers recorded were accompanied by an adult or in a household where an adult was recorded.

From: K. G <soipr@msn.com>

Sent: Thursday, April 9, 2026 1:17 PM

To: Leticia Ramirez <lramirez@thatchlaw.com>; Ryan Hooper <rhooper@thatchlaw.com>

Subject: Email of QQ Support

From: lull Mix <lullmix26k@gmail.com>

Date: Wed, Apr 8, 2026, at 2:19 PM

Subject: Quick Quack Car Wash 3815 Florin Road

2201 North View dr

Dewayne Harris

I like Quick Quack because they get your car sparkling clean in just a couple of minutes with friendly service, free vacuums, and solid unlimited plans that actually make it convenient to keep your ride looking fresh without the hassle. Plus, their fun duck vibe and “clean car guarantee” make the whole quick stop feel way more enjoyable than your average wash!

On Wed, Apr 8, 2026 at 12:04 PM Squeez Mason <[mmce33@gmail.com](mailto:mmce33@gmail.com)> wrote:

Full Name: [cameron Ellington]

**7522 Tamoshanter way**

**Sacramento California,95822**

To whom it may concern,

I support Quick Quack Car Wash because it provides fast, convenient, and high-quality service that benefits the community. It's a reliable business that keeps vehicles clean while creating a positive experience for customers.

Sincerely,

Cameron Ellington

From: **Lanii's World** <[sbrown33723@gmail.com](mailto:sbrown33723@gmail.com)>

Date: Sun, Apr 5, 2026 at 12:11 PM

Subject: Quick Car wash

To: <[soipr13@gmail.com](mailto:soipr13@gmail.com)>

I am writing to express my support for the proposed Quick Quack Car Wash project in our community.

I think that this project will benefit the area by bringing jobs, providing services needed, and contributing to local community investment.

Developments like this can help strengthen our neighborhood and offer convenient services for residents nearby, including those living near Phoenix Park.

Thank you for your time and consideration.

Sincerely,

Sky Brown

3129 Trussel Way, Sacramento, CA 95864

[Sbrown33723@gmail.com](mailto:Sbrown33723@gmail.com)

From: **Jaay ThaDonn** <[jayaawaydaa07@icloud.com](mailto:jayaawaydaa07@icloud.com)>

Date: Wed, Apr 8, 2026 at 11:57 AM

Subject: QUICK QUACK

To: <[Lilyonaparker161@gmail.com](mailto:Lilyonaparker161@gmail.com)>, <[soipr13@gmail.com](mailto:soipr13@gmail.com)>

[Jaya Wells]

[7330 Mandy drive]

I support placing Quick Quack Car Wash on Florin Road because it would bring a fast, convenient, and reliable service to the area. It would be a great addition to the community and help keep our neighborhood clean and well-maintained.



**Trevor Jones** <trevhavjoyjames@gmail.com> 12:05 PM (1 hour ago)

to me

Quick Quack is the go to method for me and my friends to keep our cars clean monthly. If you take advantage of the membership deals you can save a ton of time and money using it. I think quick quack needs to be on Floren BLVD so others can enjoy the same deal.

7958 Cottonwood Ln, Sacramento, CA 95828

**Shenise Ellington** <sheniseellington2@gmail.com> 11:52 AM (1 hour ago)

to me

Quick quack is very good for car wash at a cheap price. The customer service is Impeccable.

Shenise Ellington

From: **Christine Salas** <[statewide49@icloud.com](mailto:statewide49@icloud.com)>

Date: Wed, Apr 8, 2026 at 11:50 AM

Subject: Quick Quack

To: <[Lilyonaparker161@gmail.com](mailto:Lilyonaparker161@gmail.com)>

My name is Christine Salas and I reside at [3745 Andros Way, Sacramento CA 95823](#).

I like quick quack for the convenience of getting my car washed. Having a location on Florin Rd would be convenient for me once I got off the freeway.

Sent from my iPhone

## Appeal of Commission-Level Decision

City Code Section [17.812.060](#)

### I. Form Instructions

**Overview:** Any person dissatisfied with a Commission-level decision may appeal it to the City Council. Appeals must be filed within 10 calendar days of the decision.

**Filing Instructions:** All the following are required:

- I. **Complete Form:** Fill all portions of this form with required information.
- II. **Submit Form:**
  - Online by emailing to [planning@cityofsacramento.org](mailto:planning@cityofsacramento.org); **OR**
  - Deliver in-person to the Planning Counter at 300 Ricards Boulevard, 3<sup>rd</sup> floor.
- III. **Pay Fee.** Once this form is submitted, city staff will generate an invoice. The appeal is not considered timely filled until the fee is paid. The fee may then be paid online on the [Public Permit Portal](#) or in-person at 300 Richards Blvd, 3<sup>rd</sup> floor.
  - Appeal Fee is: \$11,115 for Project Applicants; \$1,045 for Third Party

**How to Ask Questions:** Please contact the planner assigned to the application being appealed, send an email to [planning@cityofsacramento.org](mailto:planning@cityofsacramento.org), or make an appointment to visit the Planning Counter in-person at 300 Richards Blvd, 3<sup>rd</sup> floor. To schedule an in-person visit, please use the [Appointment Scheduling](#) webpage.

### II. Appellant Information

Name: Quick Quack, Vance Shannon, Director of Entitlements

Email: vshannon@dontdrivedirty.com

Phone Number: (916) 505-8960

Mailing Address: 6020 W. Oak Boulevard, Suite 300, Rocklin CA 95765

Signature: Vance Shannon  
Digitally signed by Vance Shannon  
Date: 2026.03.04 16:20:15 -08'00'

Date: 03/04/26

### III. Appeal Information

Project Name: Quick Quack

Application Number: P25-013

Commission Decision Date: February 26, 2026

Grounds for Appeal (Attach Additional Pages, as Needed):

The Applicant appeals the Planning and Design Commission's denial of the Conditional Use Permit and Site Plan and Design review for a car wash in the Commercial (C-2) Zone. We disagree with the Commission's decision to deny the Project and maintain the approval of the Project is supported by substantial evidence in the record.